



URBAN DESIGN GUIDELINES

CITY OF PARK RIDGE

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CITY OF PARK RIDGE

URBAN DESIGN GUIDELINES

ADOPTED BY THE CITY COUNCIL
OF THE CITY OF PARK RIDGE

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INTRODUCTION

Park Ridge is distinguished by its landmark buildings, grid street system, commercial areas, parkway trees, forest preserves, parks, and neighborhoods. The qualities of these features combine to make it an attractive community and a pleasant place to live. These Urban Design Guidelines have been prepared to help architects, builders, home and business owners to maintain this character as they plan alterations to existing structures as well as new development.

Utilizing these guidelines will support in the design of structures so they can be compatible with adjacent buildings, the surrounding area, and the character of Park Ridge. Before a construction project begins, its planners should understand that the visual appearance of the property affects more than just the property involved. New and altered structures significantly impact the surrounding residential neighborhood or commercial district.

These guidelines are intended to be used by the Appearance Commission as a basis for reviewing exterior design features for projects within Park Ridge. The guidelines can also help homeowners and designers alike in their various projects.



Figure 1: Park Ridge City Hall



Figure 2: Park Ridge Presbyterian Church

ORGANIZATION OF THE GUIDELINES

The Urban Design Guidelines is divided into four parts:

- Urban Design Goals and Objectives
- Residential Guidelines
- Commercial Guidelines
- Public Space Guidelines

Part 1, “Urban Design Goals and Objectives,” specifies overall goals for community design and goals and objectives for residential, commercial, and public space development.

Part 2 is devoted to single-and multiple-family residential areas, describing methods by which residential remodeling can be made compatible with the existing structure and how new dwellings can be designed to fit into the context of the neighborhood.

The design and appearance of commercial buildings are the subject of Part 3, “Commercial Guidelines.”

Part 4 describes public spaces, with guidelines for rights-of-way, street furniture, and parks, schools, and other public buildings.

HOW TO USE THESE GUIDELINES

The following process is recommended for planning a construction project using the guidelines:

- Utilizing the Goals and Objectives to look at the community broadly, to understand its appearance.
- Determine how the property fits into the neighborhood.
- Look at the proposed construction in detail, referring to the specific guidelines for whichever type of structure is involved: residential, commercial, or public space.
- Revise the plans as needed to ensure compatibility of the new structure with its surroundings.



PART 01 URBAN DESIGN GOALS & OBJECTIVES

INTRODUCTION

A series of urban design goals and objectives was developed by the Appearance Commission. These goals and objectives are divided into three areas:

- Residential areas
- Commercial districts
- Public spaces

The following overall community goal is an expression of the community's commitment to quality visual and physical environments.

A. OVERALL COMMUNITY DESIGN GOAL

To create an identity and character that maintains and enhances the City's attractiveness, distinguishes Park Ridge from its surroundings, and supports a sense of community.



Figure 3: Devon Ave in Park Ridge



Figure 4: Shops on S Prospect Ave

B. NEIGHBORHOOD DESIGN

Residential neighborhoods are made up of private spaces, public view spaces, and public spaces. The following terms are used throughout these guidelines:

PRIVATE SPACES: Areas within and directly around homes that provide a level of personal privacy and security.

PUBLIC VIEW SPACES: Areas that share both public and private activities. Yards between the home and street, sidewalks, and parkway areas are public view because they are maintained principally by private property owners but are in full view and enjoyed by the public as a whole.

PUBLIC SPACES: Areas that include neighborhood streets, boulevards, public parks and open spaces, schools, and religious and other institutions within neighborhoods.

RESIDENTIAL DESIGN GOALS

Maintain residential neighborhoods that are attractive, distinct, and safe.

OBJECTIVES

- Achieve additions and rehabilitations that project a cohesive, unified image of the new and existing elements.
- Ensure new residential development that is in keeping with adjacent buildings and reinforces the attractive characteristics of the neighborhood.
- Promote planting and landscaping that contribute to the neighborhood character and enhance the view of private homes from the street.

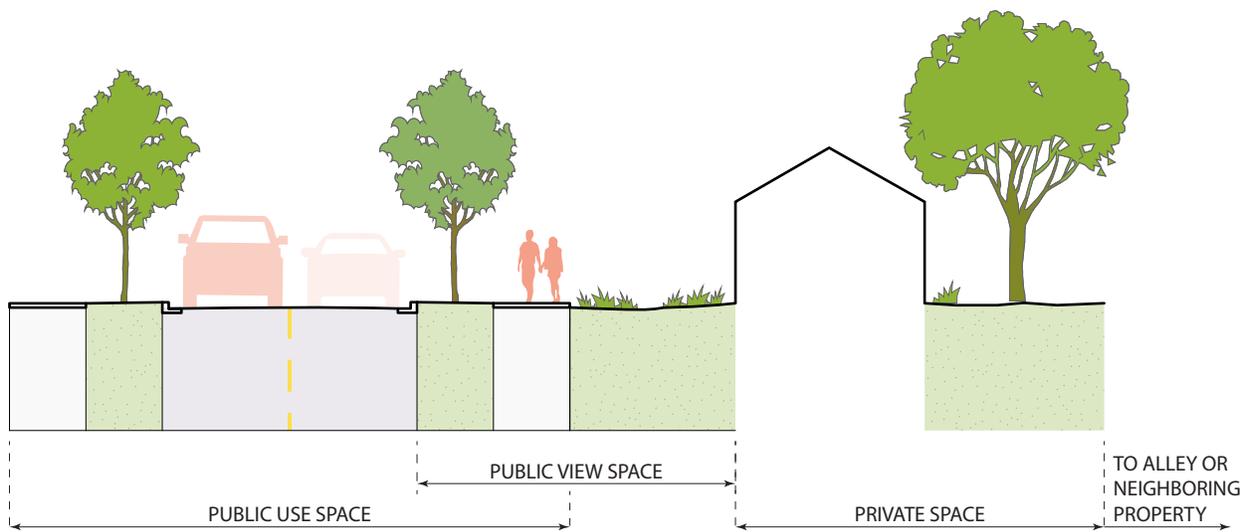


Figure 5: Relationship of Spaces for Residential Guidelines

C. COMMERCIAL DESIGN

Commercial districts in Park Ridge include well-defined retail and office areas (Uptown, South Park, and North Park) and scattered corridors (Northwest Highway, Busse Highway, Higgins Road, Touhy Avenue, and Dempster Street). Private spaces within commercial areas include structures, building courtyards, and screened outdoor areas. Public view areas include off-street parking areas and street yard open space. Public spaces include sidewalks, plazas, and on-street parking.

COMMERCIAL DESIGN GOAL

Create and maintain business districts that are attractive, convenient, and safe.

OBJECTIVES

- Encourage maintenance and/or reinvestment in existing buildings that are consistent with the character of the surrounding area.
- Preserve the integrity of buildings and spaces that express the history of Park Ridge.
- Encourage new building design that respects the historic pattern, scale, siting, and pedestrian movement patterns.
- Contribute to the overall character of the area by ensuring that all new buildings and building improvements are in keeping with adjacent buildings.
- Unify the style, scale, and appearance of all reconstruction, remodeling, and erection of visible elements on existing buildings.

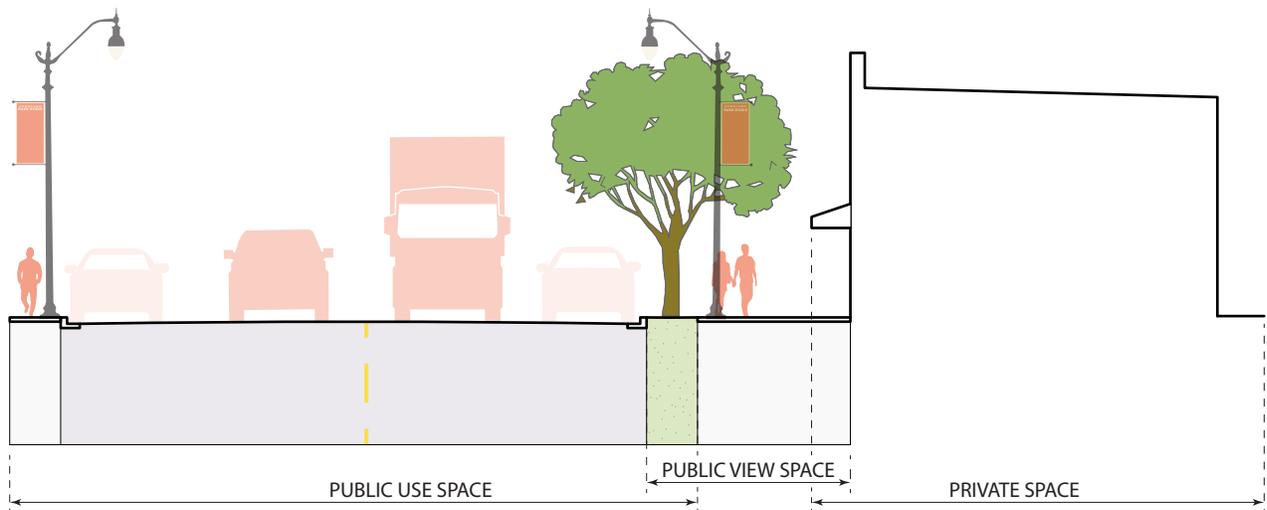


Figure 6: Relationship of Spaces for Commercial Guidelines

D. PUBLIC SPACE DESIGN

Public spaces are the areas that people see and use spanning the area from a building on one side of a street to a building on the other side. Public space design goals and objectives establish the general organization for public improvements. These elements are also a basis for private development and investment. These goals and objectives will have a major impact on maintaining and enhancing the character of Park Ridge.

PUBLIC SPACE DESIGN GOAL

Establish public improvements and amenities that unite neighborhoods and commercial areas into a unified community pattern and image.

OBJECTIVES

- Emphasize natural conditions that accentuate the character of Park Ridge.
- Enhance historic features of architecture and community layout.
- Define and improve entrances into the community.
- Encourage well-designed community and institutional buildings.

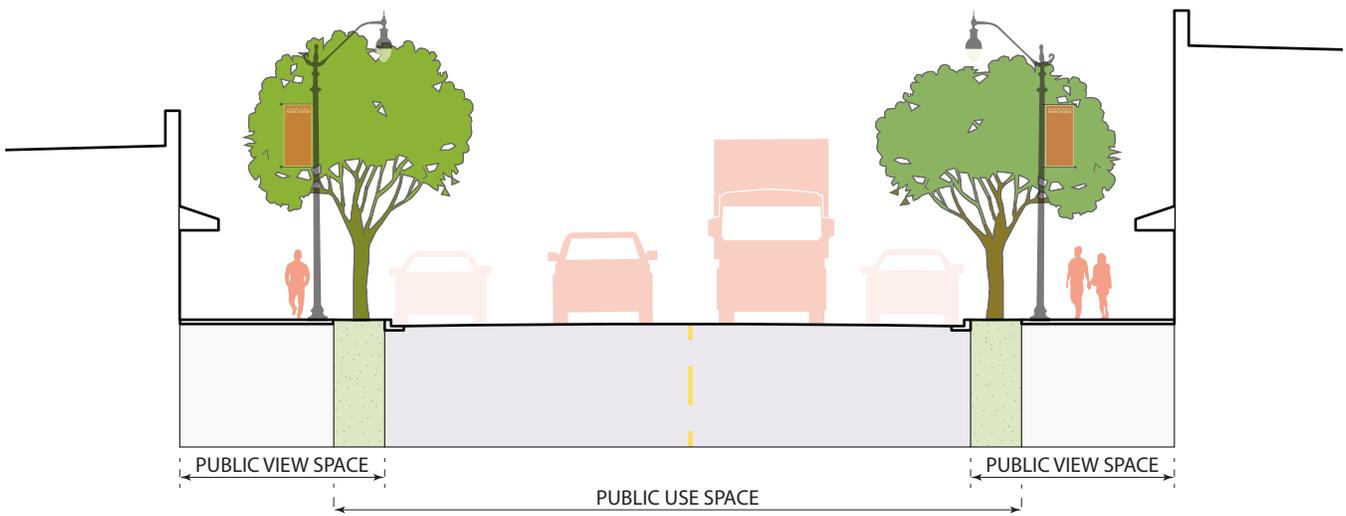


Figure 7: Relationship of Spaces for Public Space Guidelines



PART 02 RESIDENTIAL GUIDELINES



Figure 8: Apartments in Park Ridge



Figure 9: A Home in Park Ridge

Park Ridge neighborhoods are distinctive, with the following basic characteristics:

- Parkway trees consistently line most residential streets.
- Trees and shrubs within private yards lend an appearance of neighborhood maturity.
- Where landscaping is mature, neighborhoods are more attractive and have a stronger, positive image.
- The street network, including boulevards and differing street widths, contribute to a sense of variety and distinction in the Park Ridge neighborhoods.
- Most neighborhoods display a consistent character or development (through building age, materials, architectural style), but in-fill development and redevelopment have introduced variation into neighborhoods.

The primary image and identity of Park Ridge comes from its residential areas. The residential guidelines described in this part provide standards to implement the goals and objectives of the residential urban design policy. The guidelines are intended to maintain the character of Park Ridge.

They concern the following:

- Architectural styles
- Site planning
- Proportion, massing, and scale for New Construction

- Relationship of Masses for Additions
- Roofs
- Windows and Doorways
- Exterior Architectural Elements
- Surface Materials and Colors

Today, many homeowners add to or alter their homes rather than move. Homeowners have their own ideas about how to remodel homes, and it is the intent of these guidelines to ensure that the changes to homes are in harmony with the design of the original buildings and with the general appearance of the neighborhood.

Similar concerns about neighborhood character apply to new construction of single- and multi-family residences.

The guidelines assist in establishing new residences that are also in keeping with the appearance and context of their surroundings.

A. ARCHITECTURAL STYLES

There are many residential architectural styles in Park Ridge. Pages 14-18 show some of the styles typical in the community. Homeowners and developers should recognize these architectural styles and the appropriate means to respond to the style in remodeling or building projects. The approach should include both the individual building and the way the residence fits into and helps to form the image of an entire neighborhood.

These architectural styles give Park Ridge its residential character. The importance of the architectural character of the community, based on these individual architectural styles, cannot be overstated.



Figure 11: Vernacular Style

VERNACULAR STYLE

A building form native to a locale and derived from simple and direct adaptation to functional needs, using the materials and methods at the command of local builders, without regard for formal or precise stylistic notion but based on tradition and practicality.



Figure 10: Queen Anne Style

QUEEN ANNE STYLE

A style of architecture popular between 1880 and 1910 that was inspired by Medieval and Jacobean style-buildings. Queen Anne designs typically had decorative excess, and variety in the design was encouraged. These homes can sometimes include various building materials and the architects of these homes typically put greater emphasis on the second story by incorporating different materials, shapes and design.





Figure 12: Modern Style

MODERN STYLE

Modern architecture is a blend of a variety of styles from mid-20th century modern to the contemporary styles of today. Characteristics of this type of architecture include simple clean lines, open floor plans, lack of detail typically seen in historical architectural styles.

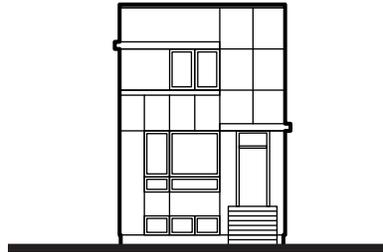


Figure 13: Cottage Style

COTTAGE STYLE

A style of architecture that originated in England during the Middle Ages. This style of architecture is characterized by small quaint homes that typically have low-pitched gable roofs and small covered porches.

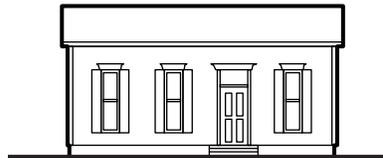


Figure 14: Farmhouse Style

FARMHOUSE STYLE

A style of architecture that originated in the 1700's. This style is characterized by asymmetrical massing with gable roofs. The farmhouse style features simple detailing and large porches.





Figure 15: Chicago Bungalow Style

CHICAGO BUNGALOW STYLE

Similar to the traditional bungalow style, Chicago style bungalows were made to fit on the City's standard 25-foot-wide lots and are distinguished by their masonry construction's ability to withstand Chicago winters.



Figure 16: Italianate Style

ITALIANATE STYLE

A style of architecture that came to prominence in the mid-1800's with stylistic elements that were derived from Italian Renaissance architecture. Typical Italianate architecture includes homes that appear taller with deep overhanging eaves coupled with decorative elements, and occasionally seen with belvederes, cupolas or towers.

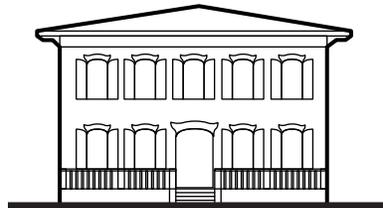


Figure 17: Shingle Style

SHINGLE STYLE

A style of architecture made unique by its presence of shingles. On these homes, shingles were used not just on the roof, but on wall surfaces. The shingle style takes after the Queen Anne style of architecture, however, does not include the decorative embellishments.





Figure 18: Georgian Revival Style

GEORGIAN REVIVAL STYLE

An architectural style reminiscent of architecture in England during the 1700s to early 1800s. This style traditionally has a symmetrical facade of punched windows on a reddish-brick building with a symmetrical plan. First seen in the United States in the New England area. Georgian revival style is like Georgian and is updated with similar characteristics.

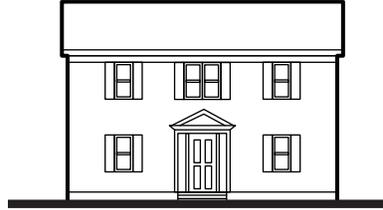


Figure 19: Bungalow Style

BUNGALOW STYLE

A style of house that is typically either one (1) story or has a second partial story built into a sloped roof. The bungalow is distinguished by its broad front porch with substantial columns on the porch rail.



Figure 20: Prairie Style

PRAIRIE STYLE

A style of architecture found in Chicago starting in 1900 by Frank Lloyd Wright and others. This style has an emphasis on nature, simplicity, and craftsmanship. This style incorporates long bands of horizontal windows, recessed entrances, long, low chimneys and the use of stucco or red brick for the exterior.

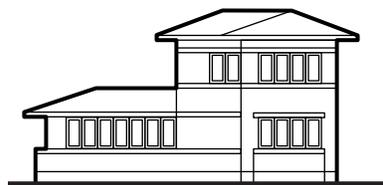




Figure 21: Ranch Style

RANCH STYLE

Long, low-lying, pitched roof structures, sometimes split level. Built primarily in the early 1950s.



Figure 22: English Tudor Style

ENGLISH TUDOR STYLE

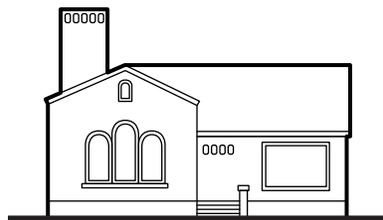
A style of architecture popular under the rule of the Tudors of England, between 1485 and 1603. Characterized by slightly rounded arches, shallow moldings, extensive paneling, exposed wood framing (generally on the upper level) with a light-colored in-fill material between the wood framing.



Figure 23: Spanish Colonial Revival Style

SPANISH COLONIAL REVIVAL STYLE

A style of architecture that incorporates simple ornamentation and was originally designed to create a uniform appearance for the beauty of the town. The style utilized building materials that were indigenous to the region. This style typically incorporates arches on entrances, interior passageways, and principal windows.



B. SITE PLANNING

B.1 SITING THE BUILDING

Two siting issues are covered in these guidelines, those include the setbacks of buildings and alleys.

I. SETBACKS

Front yard setbacks of any new construction should continue the prevailing setback patterns of adjacent buildings. Please consult [Zoning Ordinance Section 7.3](#) for yard and bulk regulations, linked here.

II. ALLEY

For the rear yard space adjacent to an alley, it is recommended to have a consistently defined edge using a hedge or fence. Hedges and fences allow for visual surveillance from the private space into the alley.

Tree canopies from the private rear yard may overhang into the alley, adding a sense of informality to what can be a stark open space.

B.2 LANDSCAPING

I. PUBLIC SPACE

Select street and boulevard trees (those in the parkways of properties) help maintain a strong sense of enclosure on a human scale. Trees throughout a neighborhood should be consistent in form and texture to create a uniform neighborhood image. The tree choice should further support the existing canopy throughout the city.

For questions regarding parkway trees, please contact the City's Forestry Department at 847-318-5231.

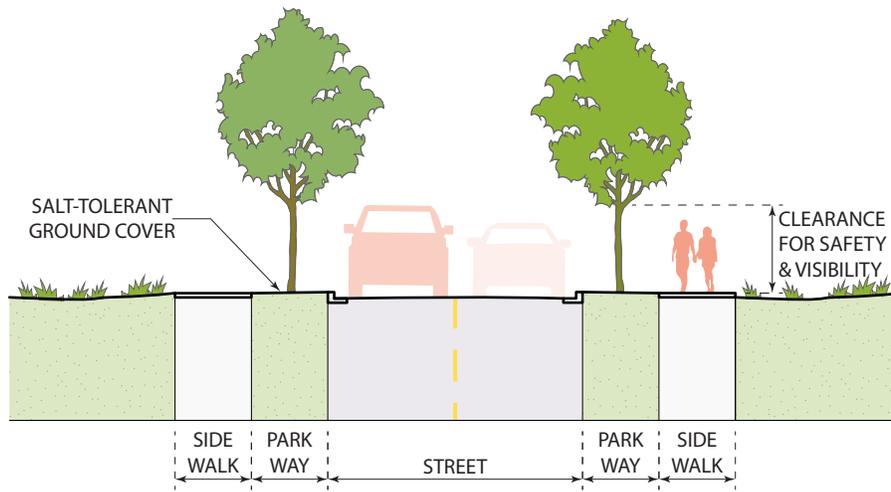


Figure 24: Elements of Residential Public Use Space

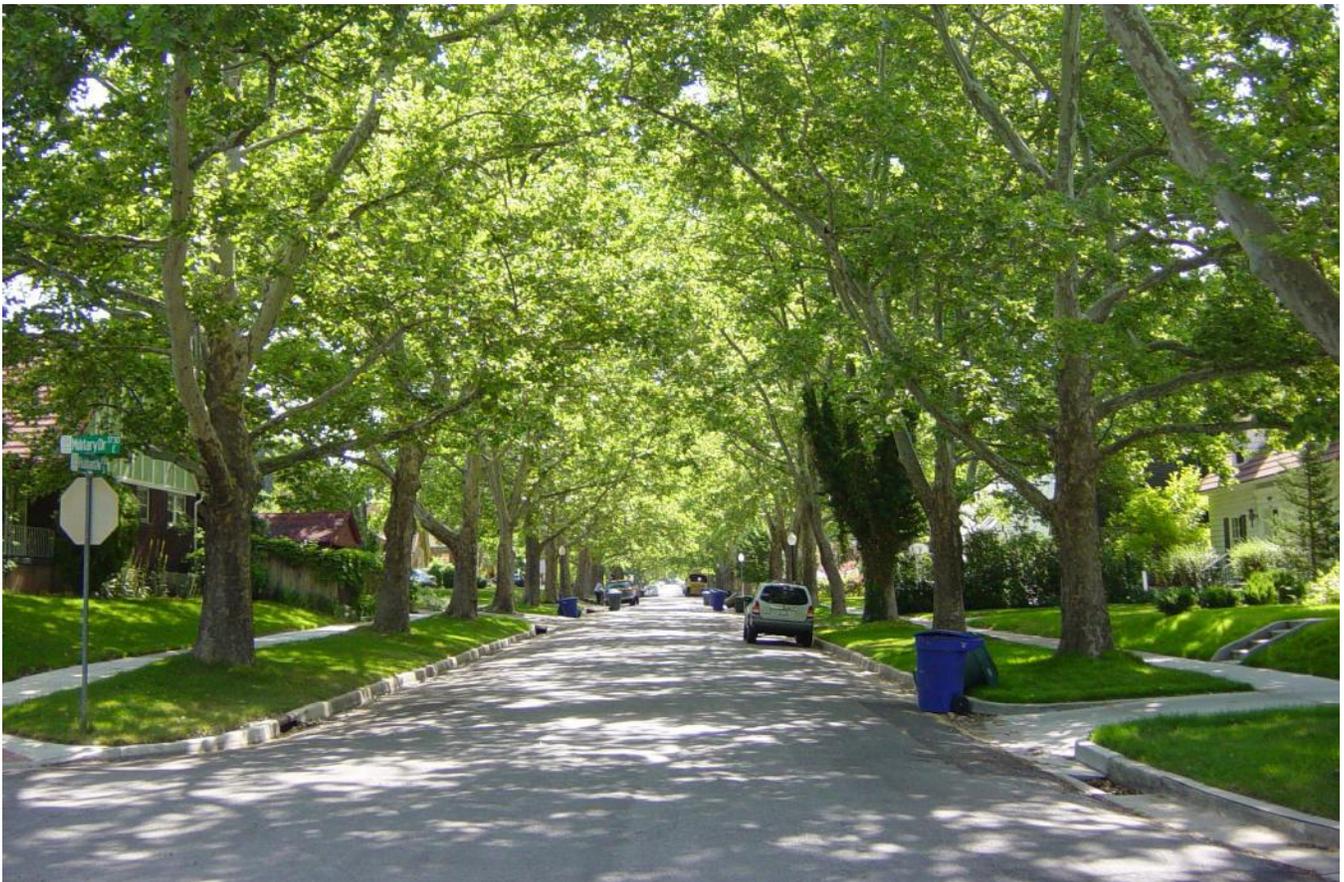


Figure 25: Street Trees Provide Sense of Enclosure

II. PUBLIC VIEW SPACE

Public view space includes the front yard and parkway. The perimeter of the property along all property lines lying between the home and the property line at the street should utilize informal landscaping. Informal landscaping would include grass, ground cover, low shrub plantings, ornamental, shade, and coniferous trees. Shrubs planted within this area should be generally short at mature height. Shade trees are preferable. Low maintenance, native plants and trees are encouraged.

Use plantings around the base of the residence and at the entryway to highlight the architectural character of the structure and guide the approach to the residence. For example, low, horizontal plants complement horizontal structures; taller, vertical plantings complement vertical structures; and formal plantings complement formal architectural styles. Consider the amount of privacy and shading that is desirable for plantings around the front entry.

To soften the harsh lines of buildings, ornamental trees are recommended to soften the appearance of those edge lines. The goal of these trees is longevity, so the Commission recommends planting at a height that will allow for a better survivability rate. The suggested height at planting would be two (2) to three (3) feet.

Lighting within residential front yards should be low intensity. Its scale should be appropriate for pedestrians, and fixtures should not cast direct light on adjoining property or the street.

Please consult the Zoning Ordinance for further specifics regarding foundation landscaping ([Section 13.7](#)) and residential lighting ([Section 11.3](#)).

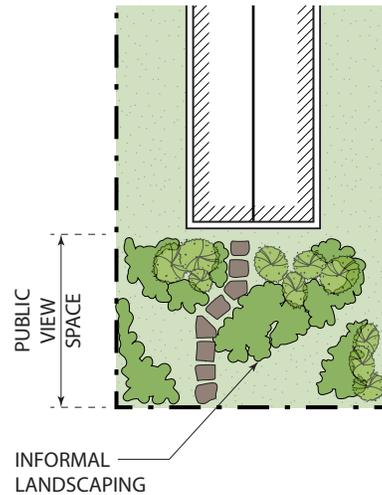


Figure 26: Public View Space Landscaping



Figure 27: Public View Space Landscaping in Park Ridge



Figure 28: Public View Space Landscaping in Park Ridge

III. PRIVATE SPACE

Private view space includes the rear yard and any service area. Private areas behind the existing building setback could be enclosed through the use of taller shrubs, fencing, and buffer plantings. Tree planting within the private area should be done in a solar conscious design, emphasizing the benefits of shade during summer months and warming sunlight in the winter.

Shrub and hedge planting along the perimeter of rear yard private spaces should emphasize privacy. Fences or the planting of hedges along alleys allow for some visual access from the private space into the alley.

Design planting within the common side yards between houses for the mutual enjoyment of adjoining properties. Property lines establish artificial boundaries for landscaping that may be avoided through common planting design.

Choose shrubs and trees according to their height so they provide visual screens between upper story windows and private activities in adjoining property. Tree height between homes can be used effectively to introduce a common scale between buildings of different heights.

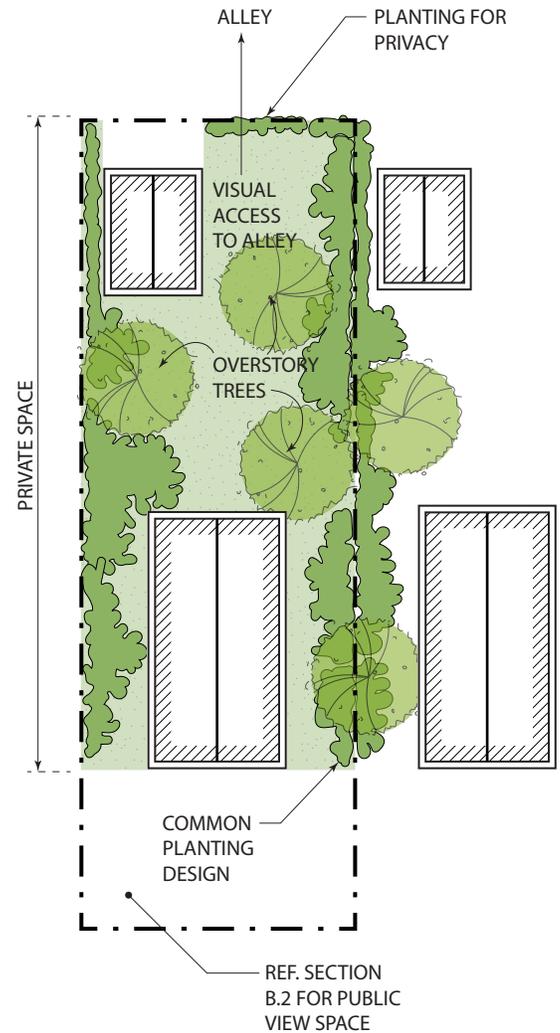


Figure 29: Elements of Residential Private Space

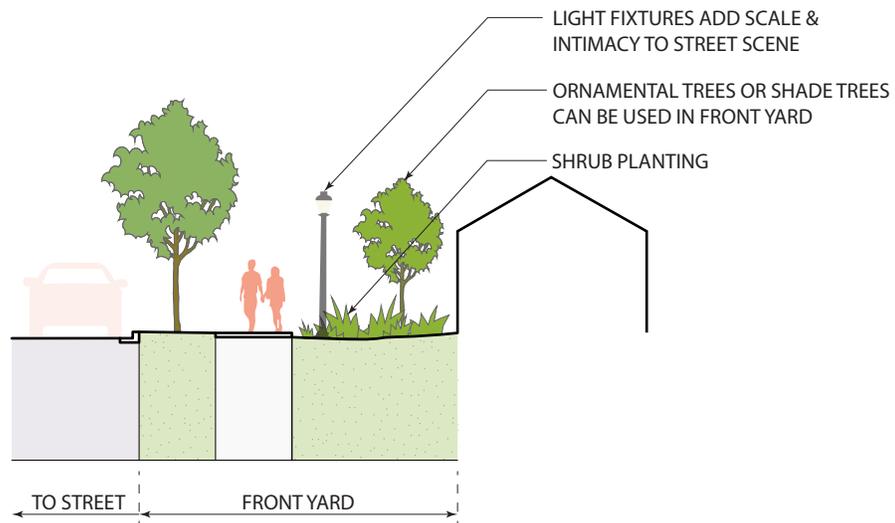


Figure 30: Elements of Residential Public View Space

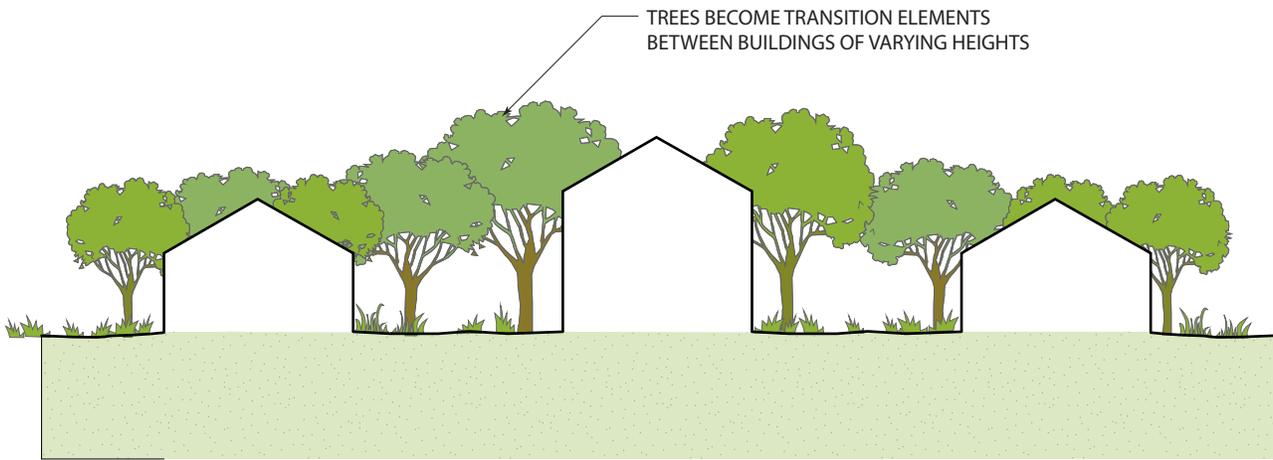


Figure 31: Side Yard Tree Planting

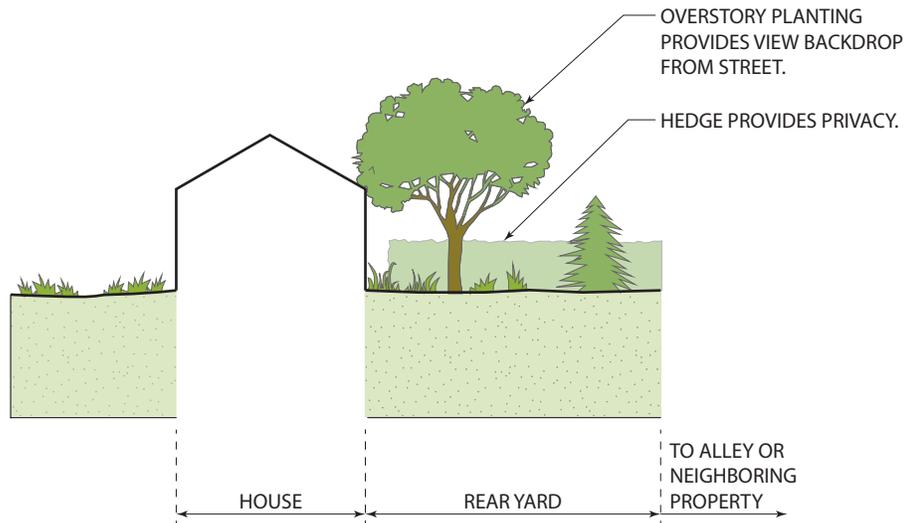


Figure 32: Elements of Residential Private Space

C. PROPORTION, MASSING, AND SCALE FOR NEW CONSTRUCTION

C.1 HEIGHT

New construction should conform to the predominant heights of roofs of nearby buildings and maintain a compatible roof form and roof line with adjacent buildings. The Commission recommends that new constructions maintain this compatibility of heights and not propose a building mass that is out of scale to neighboring homes. Consult the Zoning Ordinance ([Section 7.3](#)) for various yard and bulk regulations, including height.

C.2 SCALE

The scale and proportions of the new construction should be compatible with adjacent buildings and surrounding area.



Figure 33: Different Building Size, Similar Proportions



Figure 34: Relationship of Roof Heights

D. MASSING AND LOCATION FOR ADDITIONS

D.1 MASSING

Additions to residences should not overwhelm the original building. To create order, ensure that one mass visually dominates, with the smaller masses added to it. A conflict arises when two volumes have equal importance. One volume should dominate the total composition.

Mixed pitched roof and flat roof forms create disharmony. Ensure the addition matches the pitch of the original roof. Additions are called complementary if the forms are separated, have integrated style, respect the character of the original structure, and the scale of the addition is in keeping with the original building size. The figures to the right (Figure 35-Figure 38) show positive addition designs, while Figure 39-Figure 40 (on the following page) show conflicting addition designs.

D.2 LOCATION

When determining a location for an addition, consult the Zoning Ordinance to ensure required yards are maintained ([Section 7.3](#)). Additionally, if you are adding an accessory structure, ensure that the structure is a permitted encroachment ([Section 11.5](#)).



Figure 35: Location of Additions



Figure 36: Compatible Additive Massing



Figure 38: Integrated Addition



Figure 37: Obvious Addition

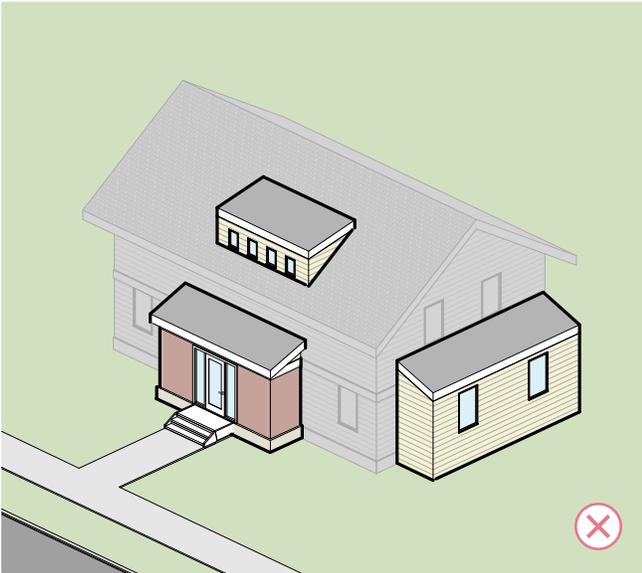


Figure 39: Mixed Forms



Figure 40: Inappropriate Addition

E. ROOFS

Roof shapes are important to defining residential architectural styles. Roof form is often the single most significant factor in determining the massing, scale, and proportions of a home. The intent of the guidelines is to have roofs compatible with the structure below and with those neighboring residences to which the visually relate.

E.1 TYPE/PITCH

Ensure that the addition's roof matches the design, angle of pitch, and materials of the original building.



Figure 41: Pitched Roof Addition

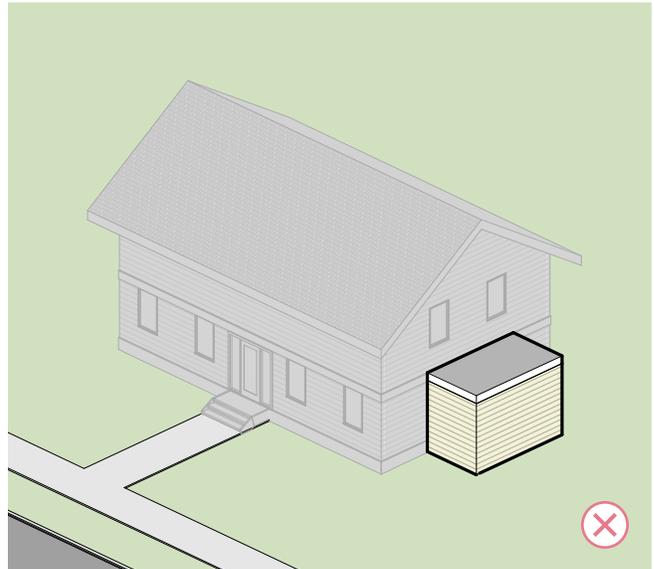


Figure 42: Inappropriate Flat Roofed Addition

E.2 DORMERS

Design dormers and windows to reflect the style, proportion, and materials of the roof and windows of the original building. Dormers should be small particularly those located at the front of the residence and should not extend to or above the ridge line of the roof.

Do not use shingles for siding or dormers. This is an inappropriate use for this material. Shingles are only to be used as roofing material.

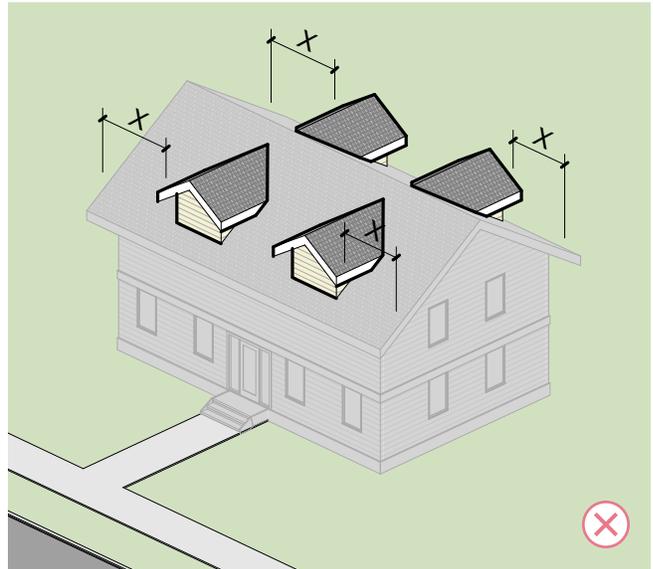


Figure 43: Dormer Setbacks

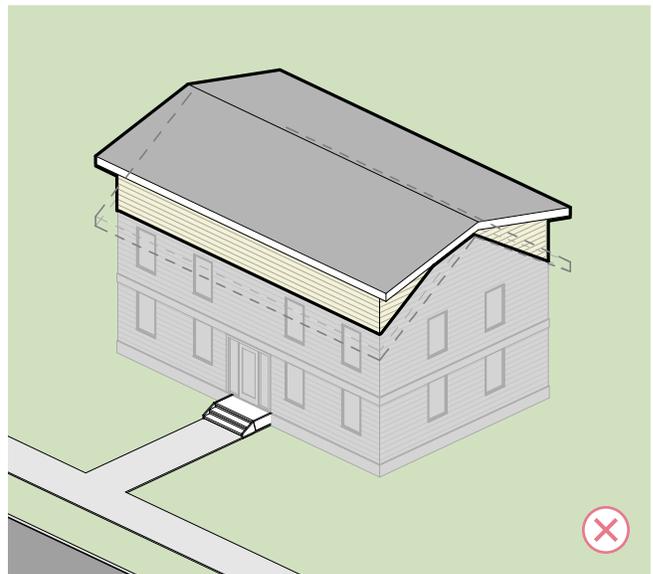


Figure 44: Second Floor Addition

E.3 SKYLIGHTS

Skylights should maintain the style of the roof. They should be proportional to the size of the roof plane, as shown in Figure 45.



Figure 45: Skylight Size and Location

E.4 SECOND- OR THIRD-STORY ADDITIONS

Often, when land on a property is limited, an upper story addition is the best way to add space to a dwelling. In planning such an addition, pay careful attention to the architectural style of the residence. In many cases, second-story additions can dramatically change the appearance of the residence and therefore change the character of the neighborhood. A common renovation mistake occurs when a second story is added without regard to the architectural philosophy of the existing design.

For example, a second-story addition on a ranch house that is surrounded by other single-story dwellings could be out of scale with the neighborhood. A way to resolve this inconsistency could be to place the second-story addition to the rear of the structure, so that it doesn't interfere with the original character and design of the residence, or negatively affect the neighborhood's character.

It is possible to add a top-story addition by using shed dormers in the attic space, such as in Figure 46.

Many times, a total top-story addition is inappropriate. The example in Figure 47 is a scaleless addition to the home that detracts from the roof line and the distinctive character of the architectural style. A simple setback on the second story addition from the edge of the roof overhang on three sides may significantly help the image and scale of the home.

Second-story additions are generally not appropriate for English Tudor and Chicago Bungalow styles. Figure 44 shows a Chicago Bungalow to which a second story addition has been inappropriately added. The type, scale, and character of the addition are also inappropriate.



Figure 46: Second Floor Addition

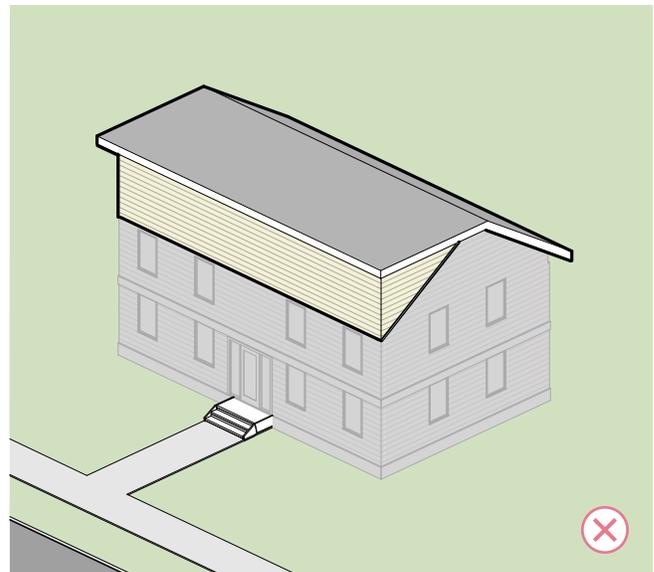


Figure 47: Inappropriate Second Floor Addition

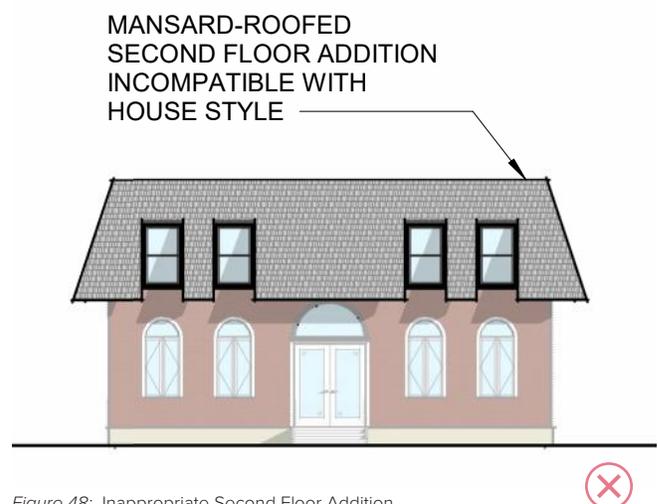


Figure 48: Inappropriate Second Floor Addition

F. WINDOWS AND DOORWAYS

F.1 RHYTHM AND BALANCE

When designing the side elevations of homes, the Commission recommends incorporating windows to break up the straight wall.

Window Colors: Vinyl, extruded wood and clad wood frame windows should be appropriately colored to match or complement the building or trim colors. Windows on the second and third floor of a building should be treated with detailing of similar quality as those on the ground floor on all sides where visible from public view. Trim should be proportionate to the size of the window. The style of trim should be consistent with the architectural style of the building.

Window shutters, if used, must be $1/2$ the width of the window opening. Design a building's windows in relation to the building's elevation, as in Figure 50. Windows in an addition should be sympathetic with the style, scale, and materials of the original building. Windows express the style of architecture by their location, proportion, length, width, and number of divided windowpanes. The rhythm of design elements, such as alternating windows and wall areas, creates interest in a building's façade. Repetitive forms created by mass and space develop a sense of order, as shown in Figure 49.

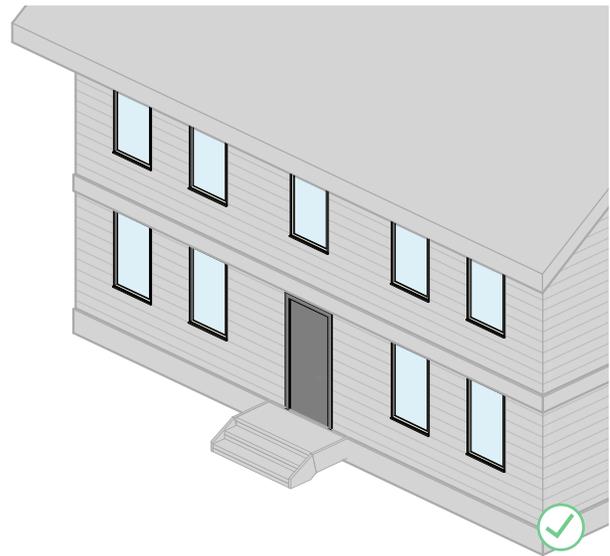


Figure 49: Rhythms of Forms

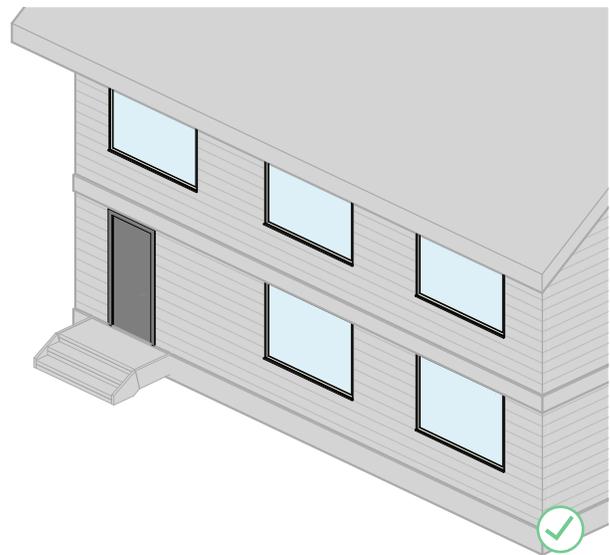


Figure 50: Similar Relationship of Windows

Figure 51 is one example of incompatible window patterns on an addition: vertical windows have been placed in the addition when the windows on the original home are horizontal. These windows compete against each another.

Figure 52 shows different sizes of windows on the same facade. This façade shows a lack of order, thought, and rhythm.

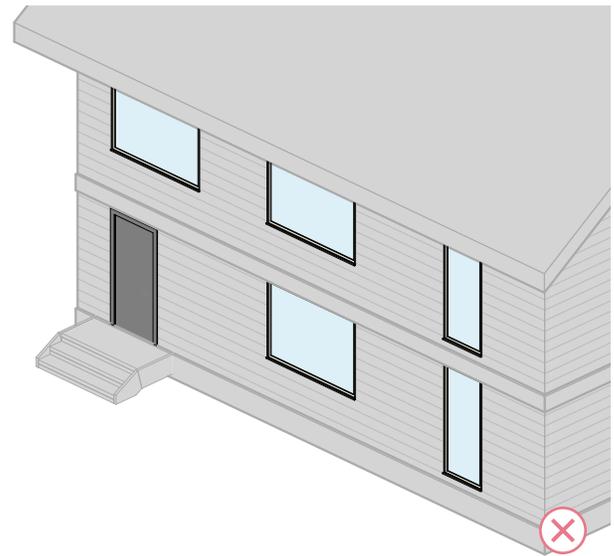


Figure 51: Competing Relationship of Windows

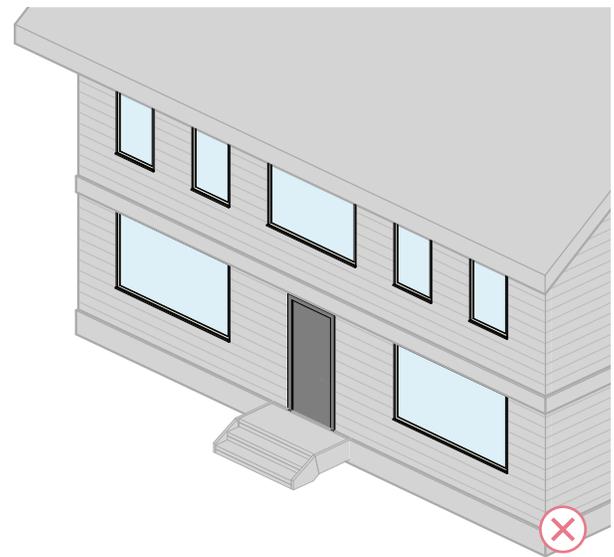


Figure 52: Incompatible Rhythm of Forms

G. EXTERIOR ARCHITECTURAL ELEMENTS

G.1 PORCHES, DECKS, AND TERRACES

Porches are structures which may be enclosed or unenclosed and are covered by roofs or eaves. Decks are raised platforms which are open to the sky. Terraces are raised impervious surface structures built on a solid base.

It is recommended to use materials and create a design for the porch or deck that extends the architectural style of the original building. Porches on the front of houses are not appropriate additions to Spanish Colonial, English Tudor, Ranch, Georgian, or Italianate residences. Treat a new wall enclosure as an in-fill wall between column supports. Match the wall to the character of the residence.

See Figure 53 and Figure 54 for appropriate porch additions.



Figure 53: Porch Addition

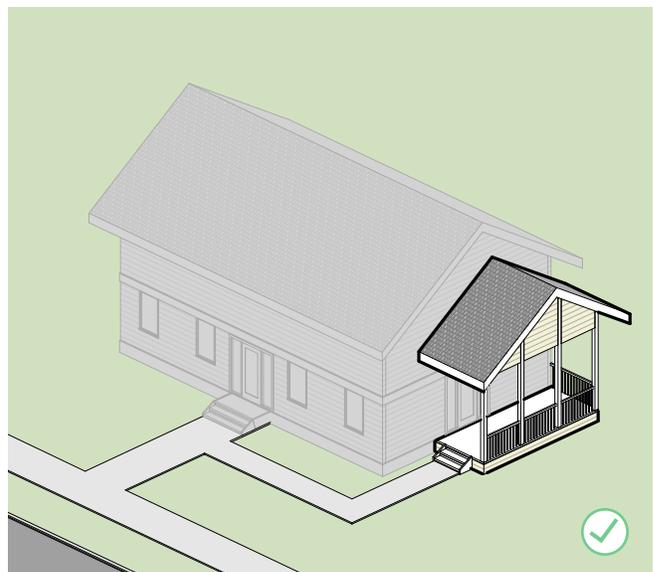


Figure 54: Porch Addition

G.2 GARAGES

In general, conceal or minimize the visual impact of garages on new construction or addition projects. Locating the garage at the rear of the lot is preferable.

If the residence will have an attached garage, regard it as an addition and coordinate its style with that of the residence. The garage should not be a dominant form of the property. When the garage is integrated with the house as a side addition, it should be set back from the primary front elevation of the residence as a secondary mass.

There are zoning requirements for attached front-loaded garages, please consult [Section 11.4](#) for additional zoning considerations when designing front-loaded garages.

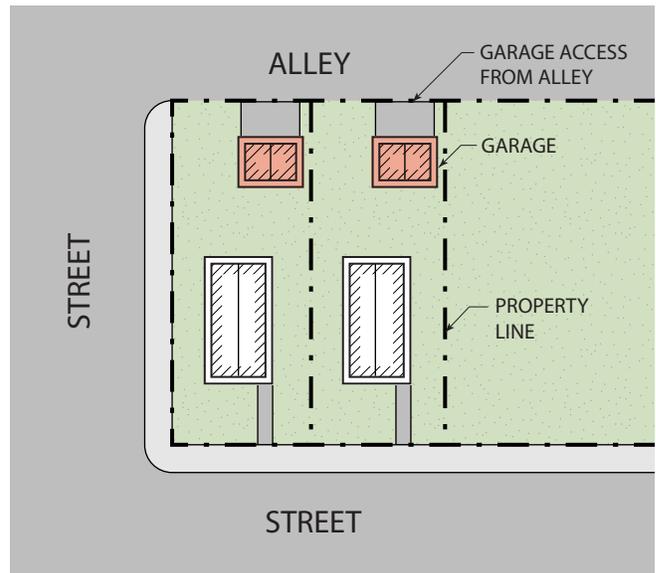


Figure 55: Garage Access from Alley

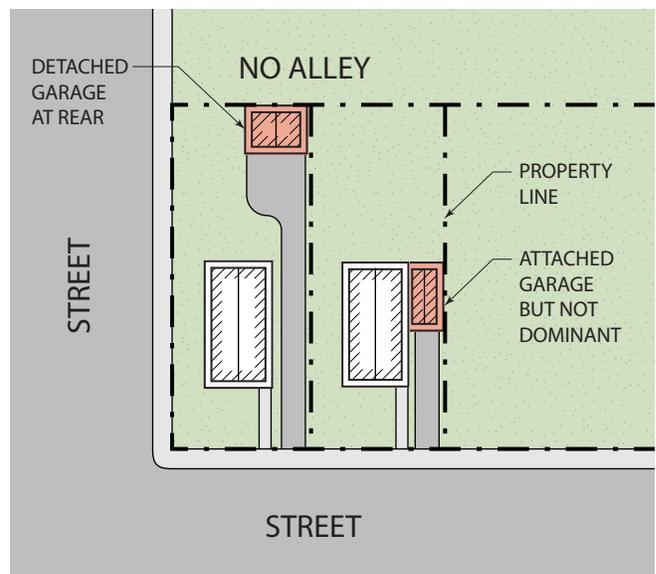


Figure 56: Garage Access from Street

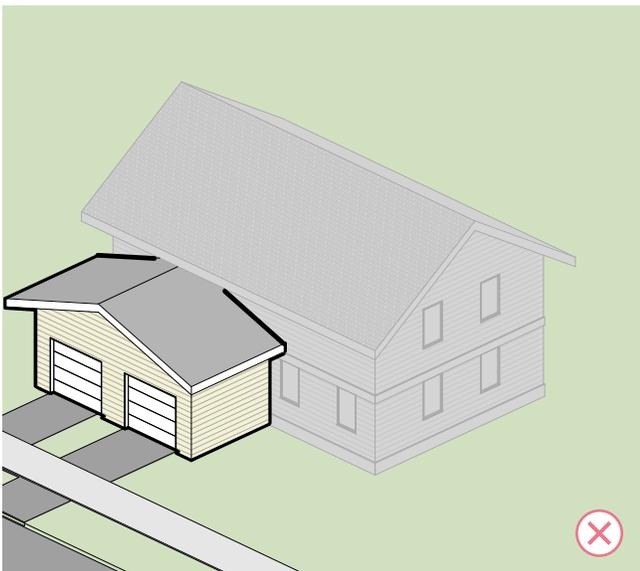


Figure 57: Inappropriate Front-Facing Garage

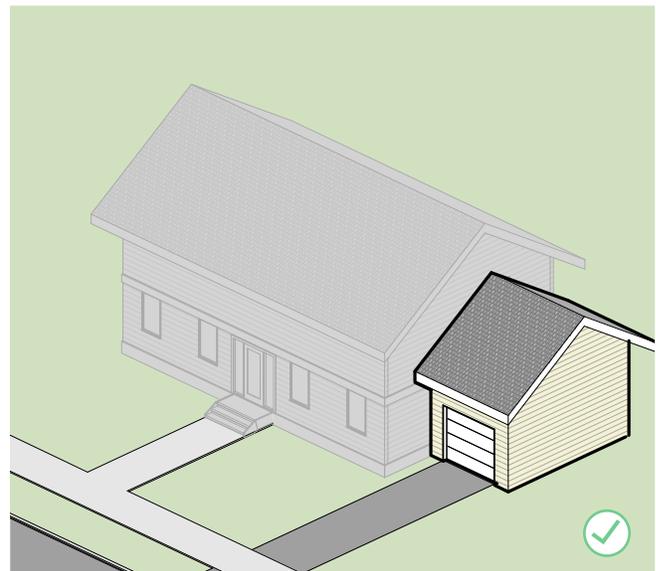


Figure 58: Appropriate Front-Facing Garage

H. SURFACE MATERIALS AND COLORS

H.1 MATERIALS

New construction that uses materials and textures similar to those of nearby buildings helps to reinforce the neighborhood's image. In time, the cohesiveness of the surroundings aids the new structure's attractiveness and appearance.

Use materials and colors for new construction that are compatible with those of neighboring buildings.

Match exterior finishes of building additions as closely as possible to the type, color, and texture of the original building. If an identical match is impossible, setback the addition six (6) to eight (8) inches to reduce the impact of the difference in finish. If a six (6) to eight (8) inch setback is unattainable, the Commission recommends breaking up the transition in some fashion. The Commission recommends this setback with new construction material changes to reduce the impact of the difference in finish.

The Commission recommends that designs not incorporate elements that create a faux front façade that differs from remaining elevations. Designs should correctly incorporate the material transitions to eliminate this effect.

Please use Figure 59 and Figure 60 for appropriate material transitions.

Do not use roof shingles for siding.



Figure 59: Change in Plane, Change in Material



Figure 60: Change in Plane, Same Material

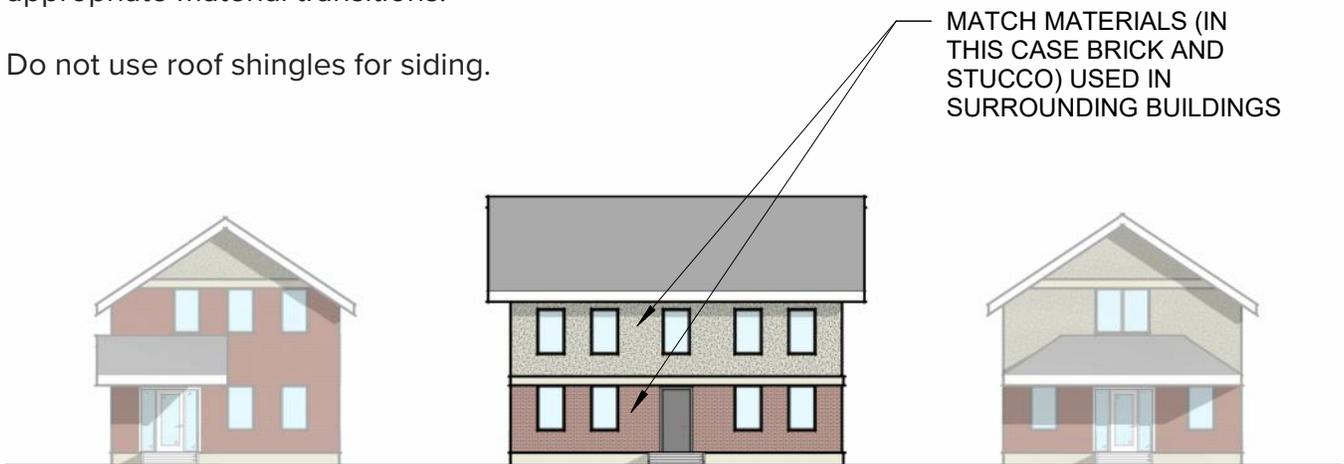


Figure 61: Relationship of Finishes

H.2 COLORS

The selection of a style-appropriate color palette along with the thoughtful application and composition of color is an important element in creating a visually attractive building as well as enhancing the character of the neighborhood.

Colors of new construction or additions should be subdued. Use light to medium colors. Bright colors should not be used as the dominant exterior color. The selection of a building's color palette should be appropriate to its architectural style.

The foundation and trim may use a contrasting color or subtle variation to the dominant color of the residence. Color blocking, or the use of multiple colors, should be used only where appropriate to the building's architectural style. Color changes should generally occur at inside corners only. Accent colors should be used primarily on ornamental elements, railings, shutters, front doors, and similar architectural features.

Masonry colors should be selected to compliment the architectural style and overall color palette of the building. Grout colors should harmonize and blend, rather than contrast with the colors of the particular masonry materials. Exposed woodwork, beams, posts, railings, etc.

should be colored to match a building's fascia. Exposed gutters and downspouts should be colored to match or compliment the surface to which they are attached or painted to match the buildings color palette.



Figure 62: Example of a simple color palette

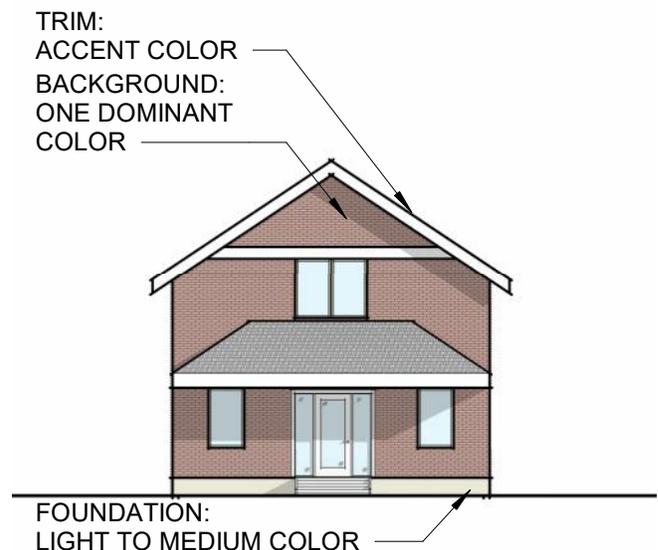


Figure 63: Residential Color



PART 03 COMMERCIAL GUIDELINES



Figure 64: Pickwick Theater



Figure 65: Public Street in Park Ridge

These commercial guidelines apply to existing, in-fill, and new developments. The goal is to design buildings that are compatible, and that create a pleasant environment in which to shop and do business. The major emphases of these commercial guidelines are the following:

- a. Public Areas
- b. Site Plan Review
- c. Parking Lots
- d. Signs

The basic principle of the commercial guidelines is to assist development in creating a pleasant pedestrian environment. This includes encouraging architectural designs that are interesting and attractive when viewed at a pedestrian scale. The guidelines strive to minimize the visual and functional impacts of automobiles.

A. PUBLIC AREAS

A.1 STREET AND RIGHT-OF-WAY WIDTHS

Maintaining an appropriate relationship of the height of buildings and the width of streets helps to create the appropriate scale and proportion of public use areas.

A.2 SIDEWALKS

Preferable sidewalk systems create a continuity-giving element that provides compatibility and harmony of the street. Use consistent sidewalk material, such as paving bricks and concrete, throughout a specific commercial area.

A.3 STREET FURNITURE

Street furniture includes benches, trash cans, mailboxes, lighting, and traffic-light equipment of compatible design within a commercial area. Well-designed street furniture is important in unifying elements of the public use zone. All of these elements are accessories located along the city streets and sidewalks and are constantly viewed by pedestrians.

A.4 LIGHTING

The function of lighting is to aid safety and security at night. The daytime look of the fixtures and the color of the light should be the primary appearance considerations in lighting selection. Lighting intensity and brightness can be scaled into a hierarchy for areas such as major streets, minor streets, pedestrian and vehicular intersections, pedestrian and area walkways, and alleyways. Match the size and scale of light poles and luminaries to the site and building scale, color, and theme.

Please consult the [Zoning Ordinance, Section 11.3](#), for exterior lighting requirements.

A.5 OFF-STREET PARKING

Parking must be convenient for customers and visitors, but its visual impact should be minimized.

Please consult [Section 12.8](#) for design standards for parking lots. Additionally, [Section 13.8](#), [Section 13.9](#), and [Section 13.10](#) provide parking lot landscaping requirements.

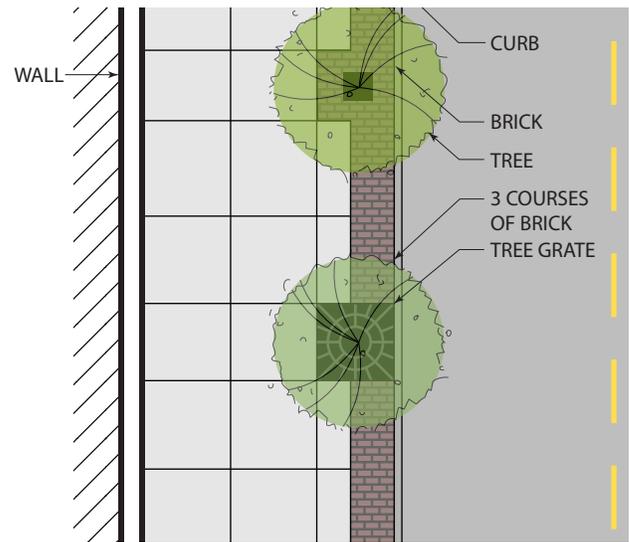


Figure 66: Typical Sidewalk Pattern

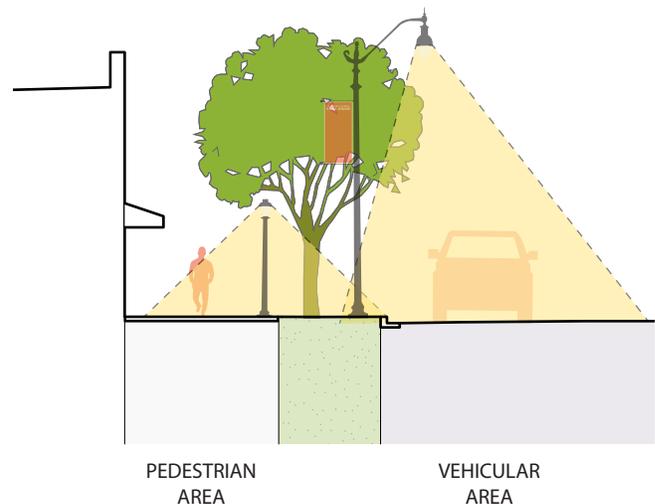


Figure 67: Lighting Scaled for Activity

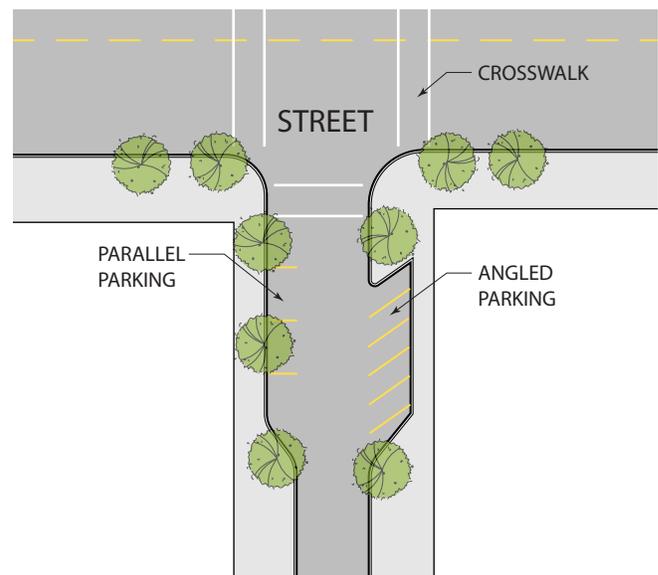


Figure 68: Parking Integrated with Streetscape

B. SITE PLAN REVIEW

Any renovation projects should work to preserve the historic integrity of the architectural style of existing buildings. New development need not copy the architectural styles of the past, new designs are acceptable. Modern designs are acceptable when the designer uses existing traditional architecture as a guide in massing proportion, scale, texture, pattern, and line.

Commercial developments will need to go through the Site Plan Review Process with the City's Planning & Zoning Commission, information regarding that process can be found on the Commission's webpage.

B.1 NEW DEVELOPMENT

Interruptions of buildings along the façade dilute the street character of commercial districts. The districts can lose much of their traditional and historic character through this interruption. These traditional values should continue to identify the character of Park Ridge.

The Appearance Commission recommends that in-fill development sites create access to parking facilities from alleys whenever possible. Arcades may be used as long as the second level of the building (and above) lines up with the existing street facade and the columns of the arcades maintain a line of store fronts. Also, pedestrian plazas along the property line are acceptable. Interesting first floor façades are preferred.

B.2 INFILL DEVELOPMENT

In-fill development should reinforce the established horizontal lines of the façades in the block. Also, the general alignment of building heights should be reinforced. Align windows with the position of other windows along the street facade.

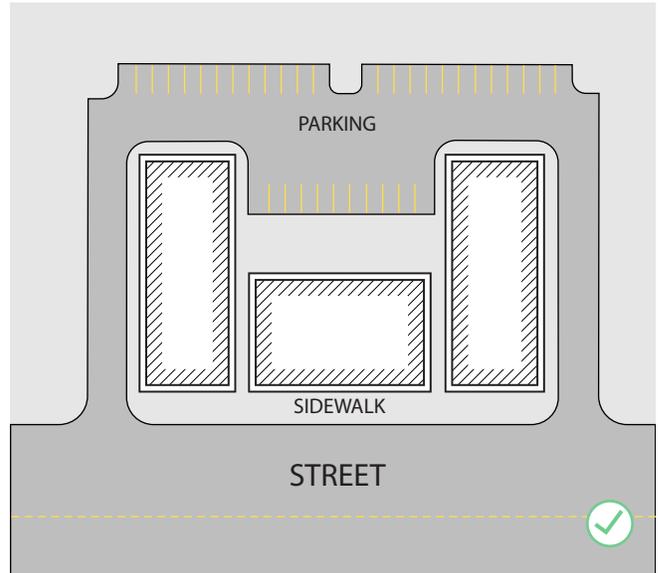


Figure 69: Relationship of Street Facades

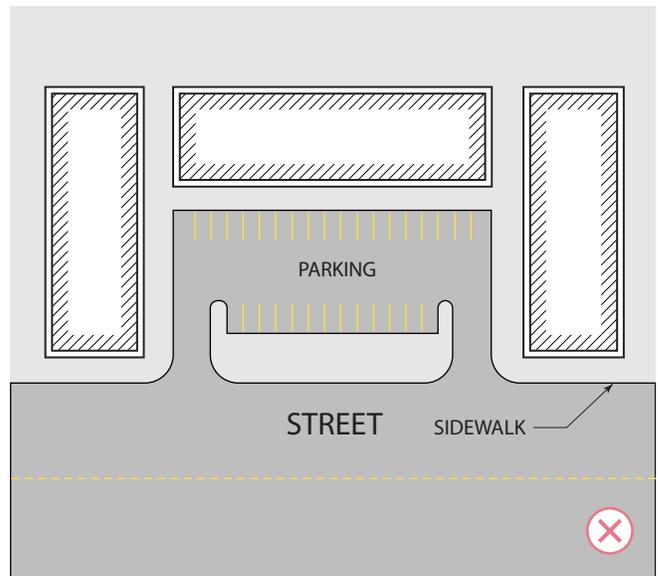


Figure 70: Relationship of Street Facades

B.3 UNIFORM BUILDING DESIGN

Further, new development in less dense areas or Park Ridge and on larger sites should incorporate design techniques that minimize the visual impact of parking from the perimeter of the property. Hide parking facilities from view of neighbors, using the building itself, fencing, and landscaping.

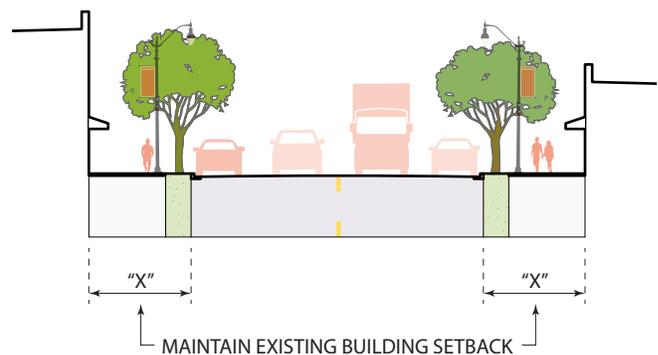


Figure 71: Uniform Building Setback

B.4 PARKING LOT LOCATION

Design and maintain appropriate landscaping in the street yard to enhance and reinforce the street view of the property.

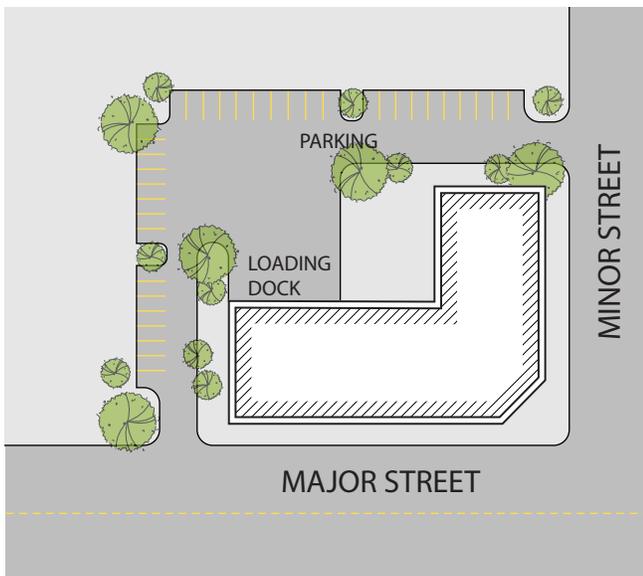


Figure 72: Parking Located Behind Building

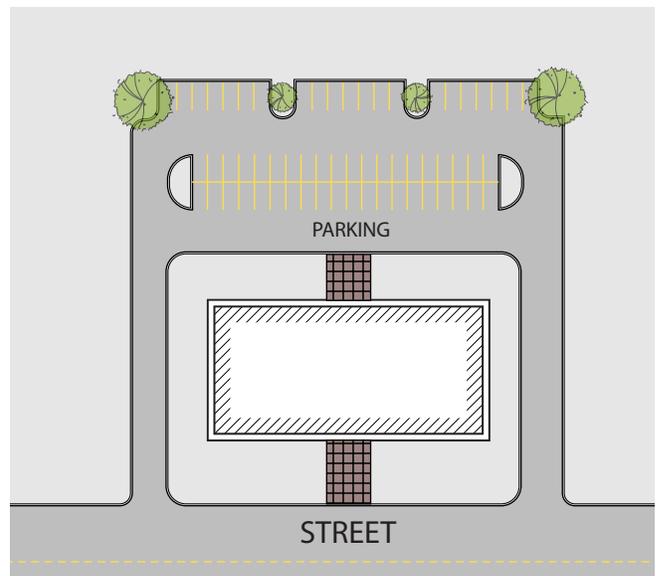


Figure 74: Parking Located Behind Building

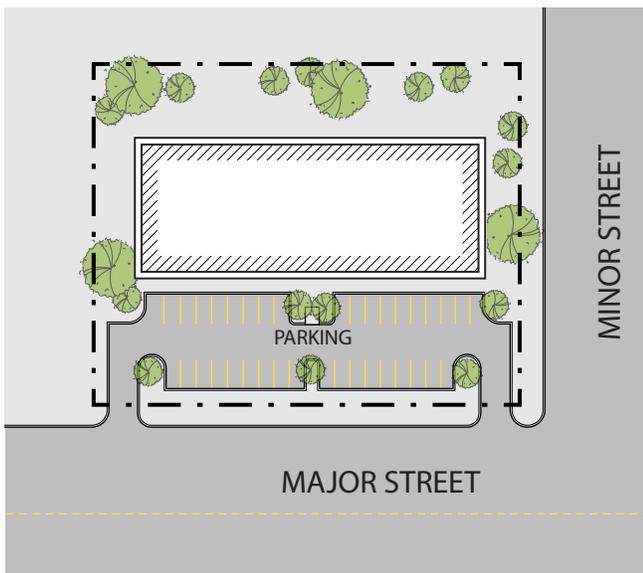


Figure 73: Parking Located in Front of Building



Figure 75: Parking Located in Front of Building

B.5 LANDSCAPING

Please refer to the [Zoning Ordinance, Section 13](#), for landscaping and screening requirements.

Please note that in the Site Plan Review process is part of the Planning & Zoning Commission's review. The development will be liable to follow said landscape plan and may receive inspections from time to time to ensure it's maintained.



Figure 76: Jersey Mikes public landscaping.

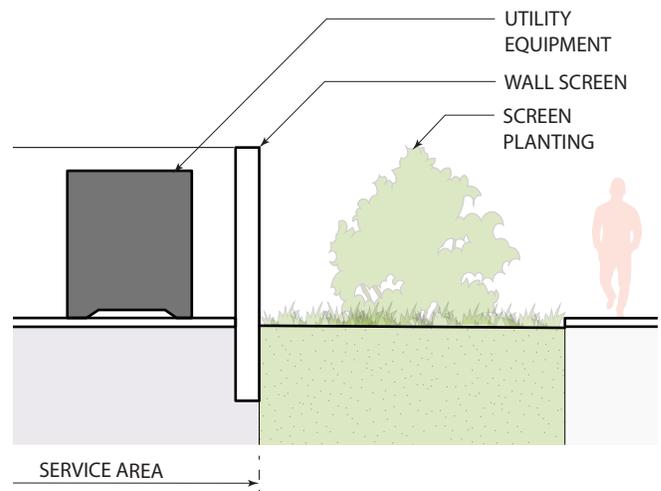


Figure 77: Service area Screen

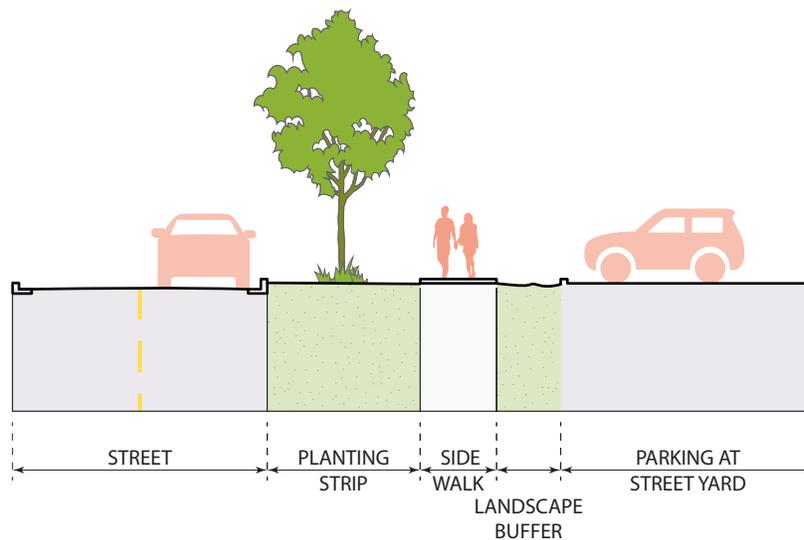


Figure 78: Parking Lot Landscape Buffer

C. PARKING LOTS

C.1 LAYOUT/LOCATION/DESIGN

Off-street parking should add appeal to the commercial property, creating clear views of entrance ways and passages through the property as well as offering convenience to patrons. This appeal results from careful planning, landscaping the lot, and minimizing its unbroken expanse of uniform hard surface.

C.2 LANDSCAPING

Please refer to the [Zoning Ordinance Section 13.8](#), [Section 13.9](#), and [Section 13.10](#) for design standards for parking lot landscaping.

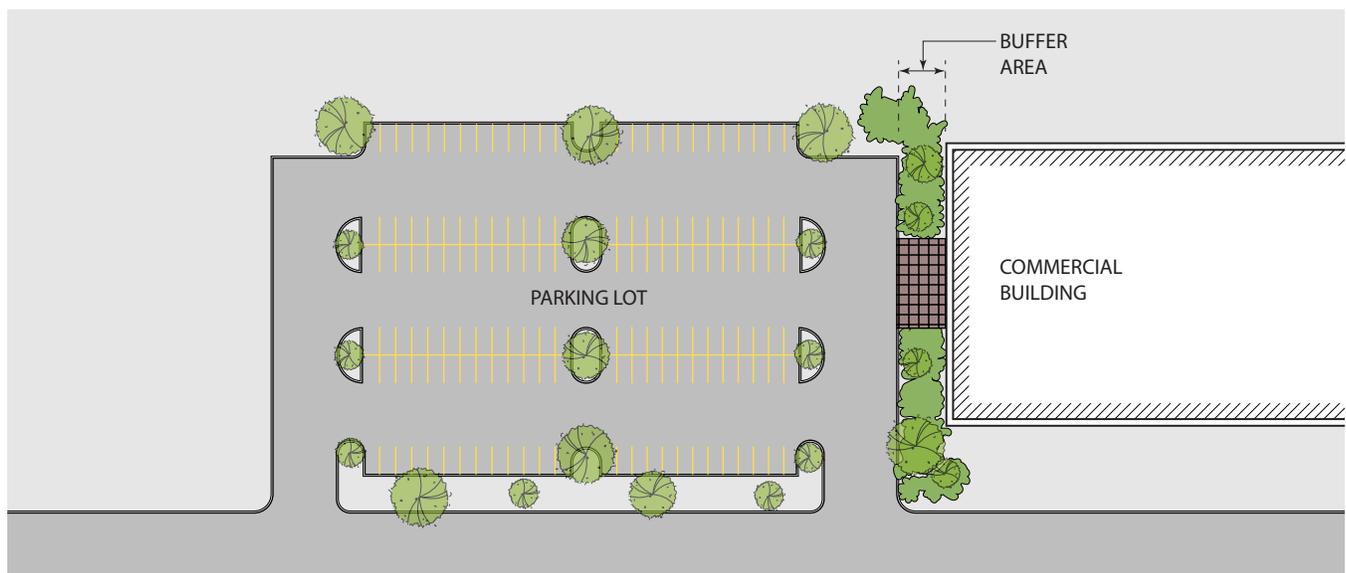


Figure 79: Parking Lot Landscaping

D. MATERIALS AND COLORS

Specific architectural styles exist in Park Ridge commercial districts. Developments should utilize materials similar in texture to those established in the commercial districts. Figure 801 shows an inappropriate selection of new building materials, and Figure 81 illustrates inappropriate use of materials in a renovation of an existing property.

The color scheme or palette should be based on traditional Park Ridge “images,” usually muted earth and natural colors. The body of the building should use such a tone. Select a compatible but contrasting color or value for the buildings trim.

D.1 COMMERCIAL GLAZING

For retail, ground floor frontages should be primarily clear, non-reflective windows that allow views of indoor commercial space or product display.

For spaces inviting the public indoors (e.g., retail, restaurants, community uses, etc.), identify opportunities to increase permeability between the sidewalk and the indoors. This may include strategies such as doors that can stay open in nice weather and making indoor activities visible from outside.

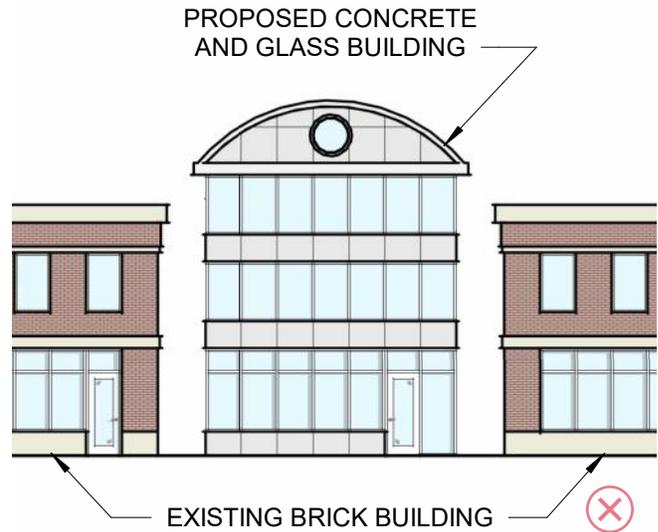


Figure 80: Relationship of Materials for In-Fill Construction



Figure 81: Relationship of Materials for Renovation



Figure 82: Building Color

E. SIGNS

Please consult [Section 14](#) of the Zoning Ordinance for sign rules and regulations.

E.1 GROUND SIGNS

Integrate ground signs into the site design and the surrounding properties. Signage design should clearly and simply convey the message.

Adapt the proportion of lettering and the number of letters to the comprehension of drivers traveling at the speed of traffic within the public right-of-way.

E.2 WALL, WINDOW, AND AWNING SIGNS

Sign lettering on these signs should suit the distance from which they will be read, contain a short message, and carry a simple design. Use a simple, easy-to-read typeface for sign lettering. Limit the mixture of typefaces to one or two type families. Create signage that is effective and equally readable for day and night viewing. To determine allowable sign size, consult [Section 14](#) of the Zoning Ordinance.

Do not apply signs to the wall or windows of the building that will interfere with architectural details or disrupt the rhythm of windows and trim.

Temporary signs in windows are not encouraged except under limited conditions. Requirements for temporary signs are found in [Section 14.6](#) of the Zoning Ordinance.

Signs displaying the building name should be legible for easy identification by both pedestrian and motorist, particularly at street corners.



Figure 83: Appropriate Signage

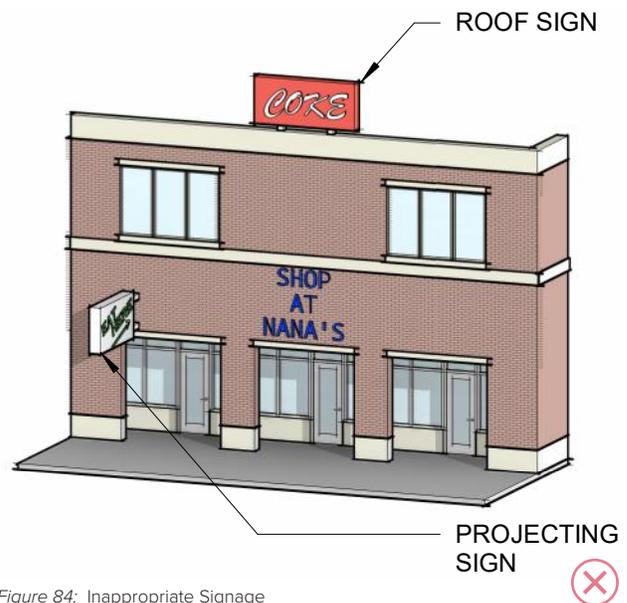


Figure 84: Inappropriate Signage

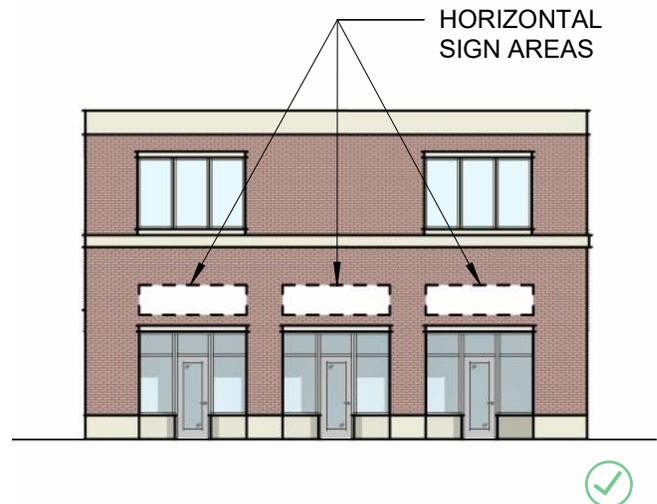


Figure 85: Location of Sign Area

E.3 AWNINGS

Awnings provide identity for a business, serve as weather protection for pedestrians, and provide shade for display areas. Awnings can be temporary or permanent structures.

Integrate the awning design with the building facade. Awnings along the street should produce a consistent pattern through their size and shape, as shown in Figure 86, but unlike the conflicting shapes shown in Figure 88. Awnings should have a compatible height-to-width relationship with surrounding awnings, be in scale with the pedestrian, and maintain a scale and proportion with the building itself.



Figure 86: Appropriate Awning

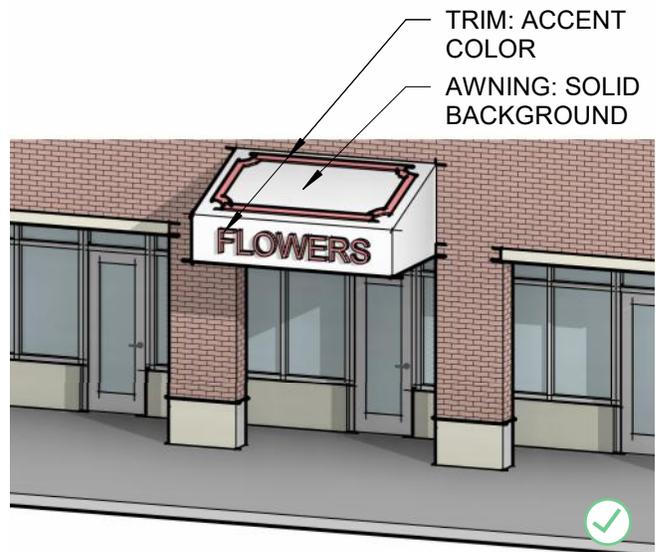


Figure 87: Awning Color



Figure 88: Inconsistent Awning Type

E.4 MATERIALS AND COLORS

The most easily legible signs are those with contrast between the sign message and the background.

The most appropriate and consistently attractive signs in the city have light-toned graphics on a dark background.

Background colors for signs should either match the building color or be both neutral and analogous to the building hue. Use light colors for lettering and darker colors for the background, as depicted in Figure 90.

It is recommended to use colors for awnings that blend with the building façade. Primary colors are not recommended for use as the awning color.

Trims may be accented with contrasting color. For example, the body of an awning should be an earth or neutral tone. The trim of an awning can be a pastel color.

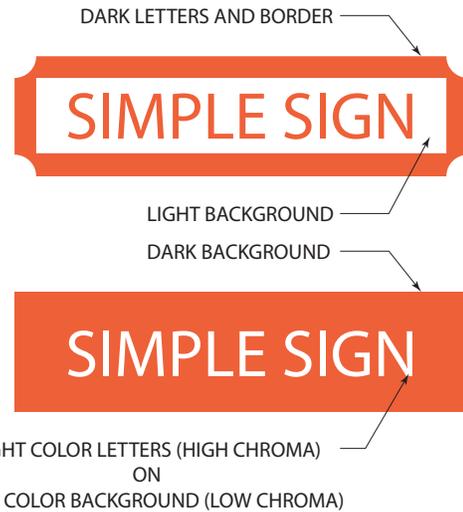


Figure 89: Signage Color



Figure 90: Commercial Signage on Prospect Ave



Figure 91: Awning Signage on Prospect Ave

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PART 04 PUBLIC SPACE GUIDELINES



Figure 92: Maine Park pollinator garden



Figure 93: Park Ridge Public Library

Public space consists of parks, plazas, schools, public buildings, and links between those spaces, such as streets, sidewalks, and rights-of-way. The design of public spaces establishes an appearance and character for Park Ridge. Their design can also unify the image of the community. The public space guidelines illustrate how to accomplish this unity.

The streetscape includes street furniture, paving, landscaping, lighting, and signage of public areas. Streetscape improvements of the City of Park Ridge can serve as catalysts for private sector development. Investments made by the City of Park Ridge to improve the public space have a major impact on the image of Park Ridge. Installation and maintenance of public space improvements for the streets and rights-of-way should enhance the special characteristics of Park Ridge.

These guidelines are grouped in the following categories:

- a. Elements of Urban Design
- b. Types of Public Properties
- c. Streetscape Elements

A. ELEMENTS OF URBAN DESIGN

Entrances, street corridors, buildings, and open spaces are the primary components that shape the urban design character of Park Ridge. Plans for public sites should coordinate the form, appearance, and arrangement of the diverse elements that make up Park Ridge.

A.1 CITY ENTRANCES AND BOUNDARIES

As major arterials approach the edge of Park Ridge, they pass through a low-density fringe of residential, commercial, and the forest preserve. City entrances should have a common theme so there is consistency throughout Park Ridge.

To enhance the image of Park Ridge at its entrances, it is recommended to plant landscaping to highlight the entries, important intersections, and neighborhoods. Combine plantings with appropriate lighting and modest signage to reflect the recognizable structure of the City and to create an attractive appearance.

A.2 TRANSIT STATIONS

Transit stations for the railroad are meeting places, waiting places, and locations where people look into the community. Their appearance is important to Park Ridge. The view from the Uptown railroad station is a major source of community image and a gateway for residents and other commuters. Additions or alterations to this area should be reflective in design of the historic and character significance of the area. Appropriate landscaping improves the appearance of the transit stations. Tall shade trees can best be used to provide passenger comfort and clear views.

A.3 PUBLIC PARKING

Off-street public parking facilities should be designed to be an attractive, inviting element of the business area. They should be designed to have a minimum impact on the visual image of the streetscape.

Please refer to [Section 13.8](#), [Section 13.9](#), and [Section 13.10](#) of the Zoning Ordinance for how parking lot and perimeter landscaping should be designed.



Figure 94: Park Ridge Metra Station



Figure 95: South Park (Devon Ave)

B. TYPES OF PUBLIC PROPERTIES

Public properties help foster a distinctive identity for Park Ridge. Distinctive development patterns in Park Ridge are created by its street system and blocks (reinforced by buildings that form continuous enclosing street walls, such as on Prospect in Uptown). Such patterns help to establish a strong, recognizable image.

B.1 CENTERS FOR CITY SERVICES

Design public improvements carefully to complement the architectural style of the public buildings, such as the City Hall and Park Ridge Library. These public buildings distinguish Park Ridge from neighboring communities. Design landscaping at city service buildings to support the dignity of public spaces and city activities.



Figure 96: Park Ridge City Hall

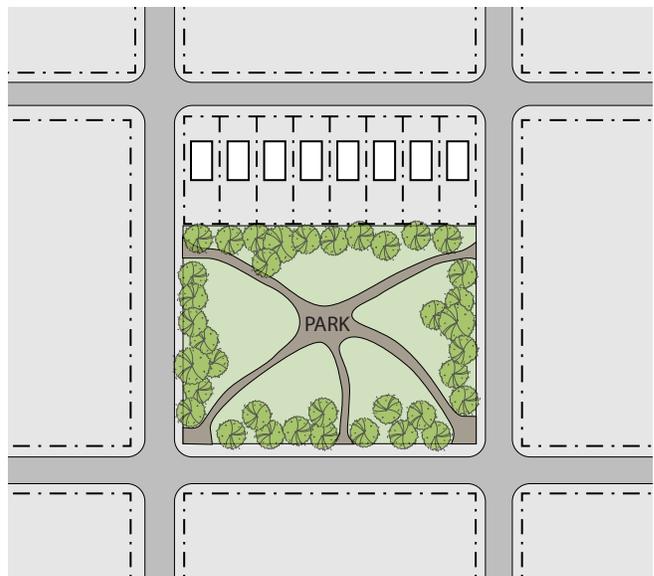


Figure 97: Park Relationship to Neighborhood

B.2 PARKS, SCHOOLS, AND OPEN PUBLIC SPACES

Please refer to the Zoning Ordinance for potential layout and design standards for such facilities. Additionally, please refer to the respective boards and governing agencies for the School and Park District.



Figure 98: Hodges Park as Village Park

C. STREETScape ELEMENTS

The principal objective in positive streetscape design involves simplicity, organization, and a minimum number or visual elements.

C.1 STREET FURNITURE

Street furniture consists of physical improvements and equipment used in outdoor spaces for purposes of security, traffic control, housekeeping, and amenity. Examples of public space street furniture are decorative fountains, public art, drinking fountains, flowerpots, tree guards and grates, bicycle storage facilities, streetlights, traffic signals and signage, posts and poles, parking meters, trash receptacles, fire hydrants, bus shelters, benches, and landscaping.

Street furniture plays an important part in the overall image of the City. Implementation of a standard for all street furniture can help create continuity throughout the community. Existing street furniture can be simply coordinated through a uniform color to assist its compatibility within the public spaces of Park Ridge. Neutral or earth tones are generally most appropriate for street furniture.

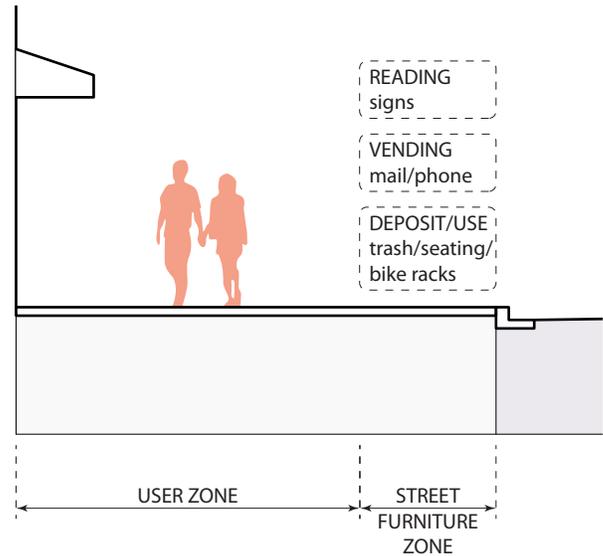


Figure 99: Elements of the Streetscape



Figure 100: Streetscape Furniture

C.2 PAVING AND BUILDING MATERIALS

Pavement surfaces can aid compatibility of public space elements through a consistent use of materials. When designed and installed well, paving can add a substantial sense of uniformity to the public and adjacent private spaces.

Building materials for public buildings should use a consistent scheme of materials and colors, as well as implement a cohesive design. Design additions to public buildings using compatible materials and using the architectural forms prevalent in the existing building massing, form, and rhythm, Figure 102.

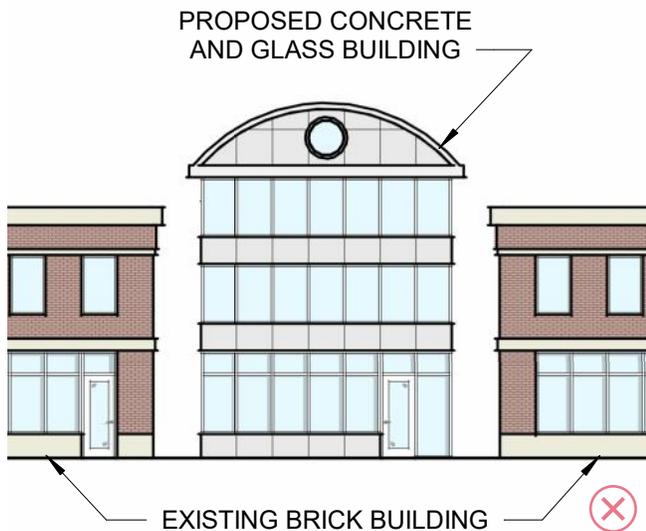


Figure 101: Relationship of Building Materials With Existing Context

C.3 LANDSCAPING

The importance of maintaining and reinforcing the pattern of a canopy of large shade trees around public spaces cannot be overemphasized. Although most private properties in the residential areas contain additional trees, it is primarily the trees within the public rights-of-way and parks that produce the positive influence of mature canopies of trees, as shown in Figure 102.

Landscape design should seek to maintain a consistent pattern of shade trees along all residential and commercial streets. Street trees provide a sense of formality, tradition, and peacefulness, and enhance the resident's or visitor's ability to distinguish Park Ridge from surrounding communities.



Figure 102: Cathedral Arch Formed by Parkway Trees



APPENDIX 01 GLOSSARY



Figure 103: Pickwick Theater

AWNING: A temporary or permanent covering extending from a building to over the sidewalk or entrance; usually made of a fabric-type material.

BUNGALOW: A style of house that is typically either one (1) story or has a second partial story built into a sloped roof. The bungalow is distinguished by its broad front porch with substantial columns on the porch rail.

CHICAGO BUNGALOW: Similar to the traditional bungalow style, Chicago style bungalows were made unique to fit into standard size city lots and their ability to withstand the Chicago winters.

COLUMN: A long vertical structural member that supports a load. In classical terms, a cylindrical support having a base, shaft, and capital.

COMPATIBLE: Capable of existing together harmoniously, in agreement

CORRIDORS: Major vehicular and railroad routes through the city.

COTTAGE STYLE: A style of architecture that originated in England during the Middle Ages. This style of architecture is characterized by small quaint homes that typically have low-pitched gable roofs and small covered porches.

DECK: An unroofed level surface and its supporting members, attached to or made part

of a building to create an exterior living space.

DORMER: A roofed structure with a vertical window that projects from a pitched roof.

EDGE: A border or boundary.

ELEVATION: The view of a side of a building. An accurate drawing of one side of a building that represents its true dimensions in the planes perpendicular to the line of sight.

ENGLISH TUDOR STYLE: A style of architecture popular under the rule of the Tudors of England, between 1485 and 1603. Characterized by slightly rounded arches, shallow moldings, extensive paneling, exposed wood framing (generally on the upper level) with a light-colored in-fill material between the wood framing.

FACADE: Exterior front face of a building. Part of a building (facing a street, courtyard, and so on); usually the most ornate elevation.

FARMHOUSE STYLE: A style of architecture that originated in the 1700's. This style is characterized by asymmetrical massing with gable roofs. The farmhouse style features simple detailing and large porches.

GABLE: A vertical triangular shape of a building wall above the cornice line of a roof formed by two sloping roof planes.

GATEWAY: A means of entrance or access.

GEORGIAN STYLE: An architectural style reminiscent of architecture in England during the 1700s to early 1800s. This style traditionally has a symmetrical facade of punched windows on a reddish-brick building with a symmetrical plan. First seen in the United States in the New England area. Georgian revival style is like Georgian and is updated with similar characteristics.

GROUND SIGNS: Signs that are free-standing on the ground and supported by some

structural element from the ground, such as a pole or wall.

ITALIANATE: A style of architecture that came to prominence in the mid-1800's with stylistic elements that were derived from Italian Renaissance architecture. Typical Italianate architecture includes homes that appear taller with deep overhanging eaves coupled with decorative elements, and occasionally seen with belvederes, cupolas or towers.

LOCAL ARCHITECTURE (or VERNACULAR ARCHITECTURE): A building form native to a locale and derived from simple and direct adaptation to functional needs, using the materials and methods at the command of local builders, without regard for formal or precise stylistic notion but based on tradition and practicality.

MASS: A simple, three-dimensional building volume. A solid object that occupies space.

MASSING: A combination or several masses to create a building volume organization of the shape or a building, as differentiated from wall treatments, windows, and so on.

MODERN STYLE: Modern architecture is the blend of modern styles that include a variety of styles. Characteristics of this type of architecture include simple clean lines, open floor plans, lack of detail typically seen in historical architectural styles.

MIDCENTURY MODERN: Midcentury modern differs from contemporary modern with the addition of natural elements, as well as the creation of depth through varying heights of walls and cabinets.

PALETTE OF MATERIALS: The spectrum or series of materials used within the design guidelines.

PITCH: The slope of a roof, usually expressed

as a ratio of vertical rise to horizontal run (x" vertical to 12" horizontal).

PLAN: A two-dimensional view of a building, or horizontal section of it seen from above. A precise drawing showing the arrangement of the design, including wall openings and dimensions.

PORCH: A structure attached to a building to shelter an entrance: a semi-enclosed space, usually roofed and open-sided.

PRIVATE SPACE: Area within and directly around homes that provide some level of personal privacy and security.

PROPORTION: The relation of one dimension to another, usually described as a numerical ratio. In architecture, proportion can determine height to width.

PUBLIC SPACES: These include neighborhood streets, boulevards, parks, open spaces, religious and other institutional open space areas.

PUBLIC VIEW SPACE: Spaces that share both public and private activities; yards between homes, street yards, sidewalks, and parkway areas. Maintained principally by private property owners but in full view of the public.

QUEEN ANNE: A style of architecture popular between 1880 and 1910 that was inspired by Medieval and Jacobean style-buildings. Queen Anne designs typically had decorative excess, and variety in the design was encouraged. These homes can sometimes include various building materials and the architects of these homes typically put greater emphasis on the second story by incorporating different materials, shapes and design.

RANCH STYLE: Long, low-lying, pitched roof structures, sometimes split level. Built primarily in the early 1950s.

SCALE: The relationship between the apparent size of a building and the size of a human being.

SHINGLE: A style of architecture made unique by its presence of shingles. On these homes, shingles were used not just on the roof, but on wall surfaces. The shingle style takes after the Queen Anne style of architecture, however, does not include the decorative embellishments.

SIDING: Boards applied to an exterior wall, each of which overlaps or meets the one below it to create a continuous skin over the wooden frame.

SITE PLAN: An accurately scaled drawing of a site (lot or parcel) as if seen from above, describing the property, boundary, orientation and location of buildings, driveways, walkways, and other constructed site improvements. Includes vegetation, new plantings, and contour intervals.

SKYLIGHT: A glazed opening in a roof plane that admits light.

SPANISH COLONIAL: A style of architecture that details simple ornamentation and was originally done to create a uniform appearance for the beauty of the town. The style utilized building materials that were indigenous to the region. This style typically incorporates arches on entrances, interior passageways, and principal windows.

STREET FURNITURE: Elements of street improvements that are additive elements such as light fixtures, benches, trash cans, and planters. The term is applied to physical improvements and equipment used in outdoor spaces used for purposes of security, traffic control, housekeeping, and amenity.

STREETSCAPE: Architectural forms, details, materials, colors, signs, and street furniture

that are orchestrated to create characteristics of a street scene.

STREET WALL: Any wall fronting a street. A Street Wall Line extends outward, from the outermost points of each building's street wall, parallel to the street, until such extensions of said line intersect the side and/or rear property line encircle the building or intersect another wall line. (If a building has rounded front or the building is on an irregular shaped lot, the points of the street wall closest to the side property lines shall be used to determine the street wall line.)

TEXTURE: The arrangement of particles or constituent units of any material or grouping as it affects the appearance or feel of its surface or context.

URBAN DESIGN FRAMEWORK: The organization of spaces and the hierarchy of those spaces.

VEHICULAR USE AREAS: All areas subject to vehicular traffic including accessways, driveways, loading areas, service areas, and parking stalls but excluding covered parking structures and underground parking.



APPENDIX 02 PHOTO CREDITS



Figure 104: Bank building on S Northwest Highway

Figure 1 (p.06):

City of Park Ridge Facebook Page

Figure 2 (p.06):

Park Ridge Presbyterian Church

<http://www.parkridgepresby.org/>

Figure 3 (p.08): - Figure 4 (p.08):

City of Park Ridge

Figure 8 (p.12):

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Figure 9 (p.12):

Redfin

www.redfin.com

Figure 10 (p.14):

Chicago Magazine

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Figure 11 (p.14): - Figure 23 (p.18):

Indiana Landmarks and Historic Preservation

www.indianalandmarks.org

Figure 25 (p.20):

City of San Jose

www.sanjoseca.gov

Figure 27 (p.21):

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Figure 28 (p.21):

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Figure 59 (p.34):

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www.diychatroom.com

Figure 60 (p.34):

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Figure 64 (p.36): - Figure 65 (p.36):
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Figure 76 (p.41):
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Park Ridge Park District
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Figure 93 (p.48):
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Figure 100 (p.51):
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Figure 102 (p.53):
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www.cnu.org

Figure 103 (p.54): - Figure 105 (p.60):
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APPENDIX 03 REFERENCED MATERIALS



Figure 105: Park Ridge Metra Station

City's Forestry Department: 847-318-5231

Zoning Ordinance [Section 7.3](#) for yard and bulk regulations.

Zoning Ordinance [Section 11.3](#) for exterior lighting requirements.

Zoning Ordinance [Section 11.4](#) for zoning considerations regarding attached front-loaded garages

Zoning Ordinance [Section 11.5](#) for permitted encroachments of accessory structures.

Zoning Ordinance [Section 12.8](#) for design standards for parking lots.

Zoning Ordinance [Section 13](#) for landscaping and screening requirements.

Zoning Ordinance [Section 14](#) for sign rules and regulations.